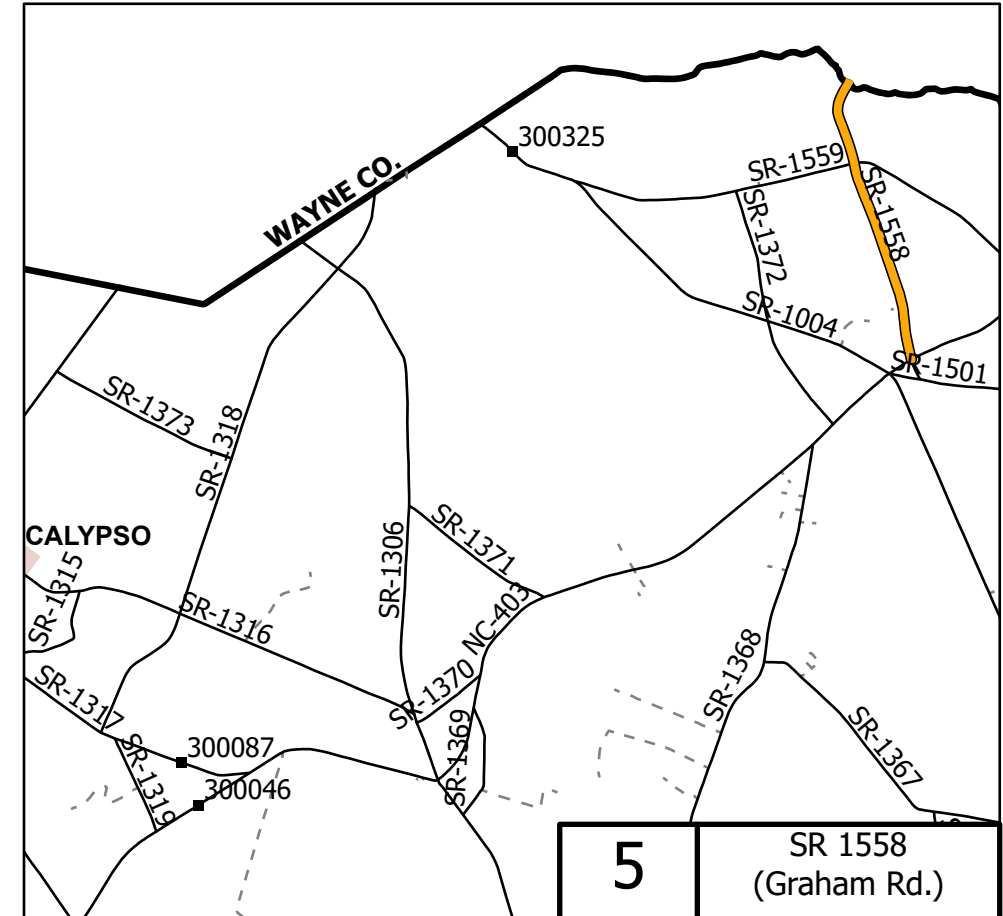
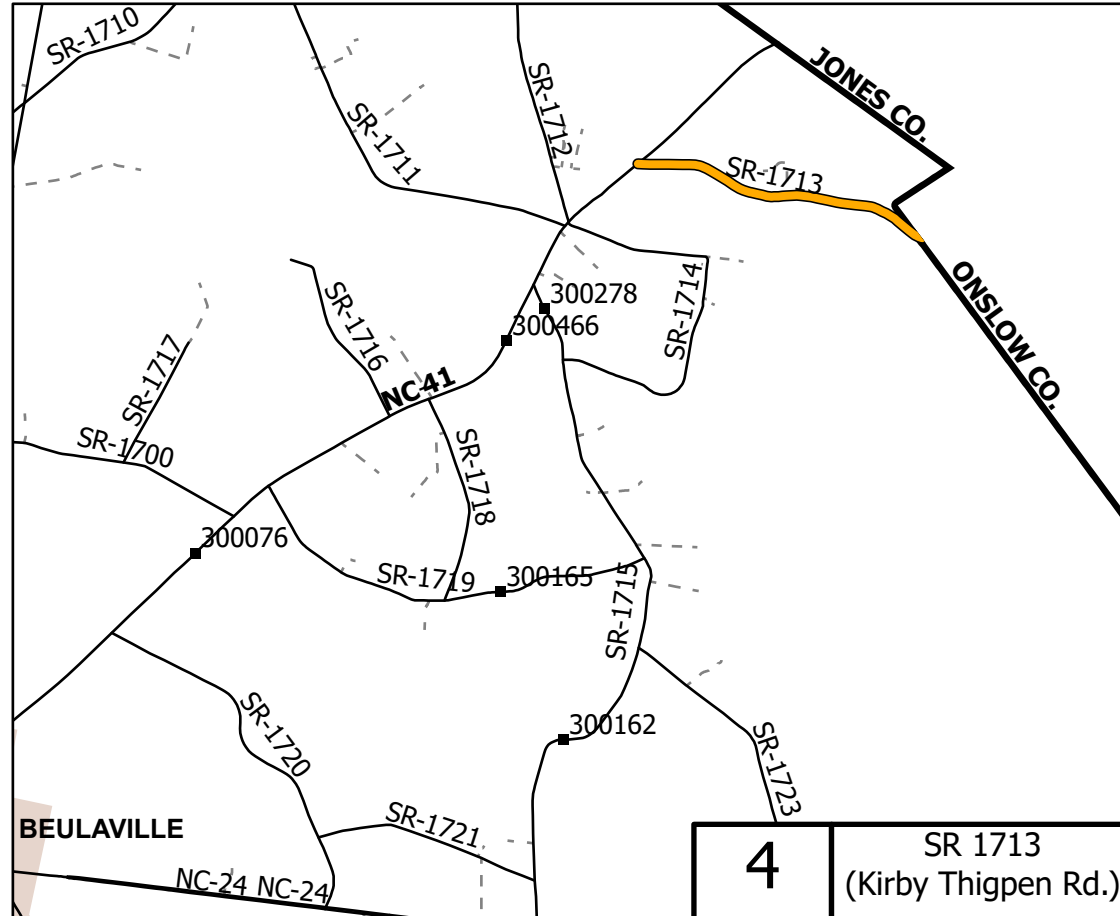
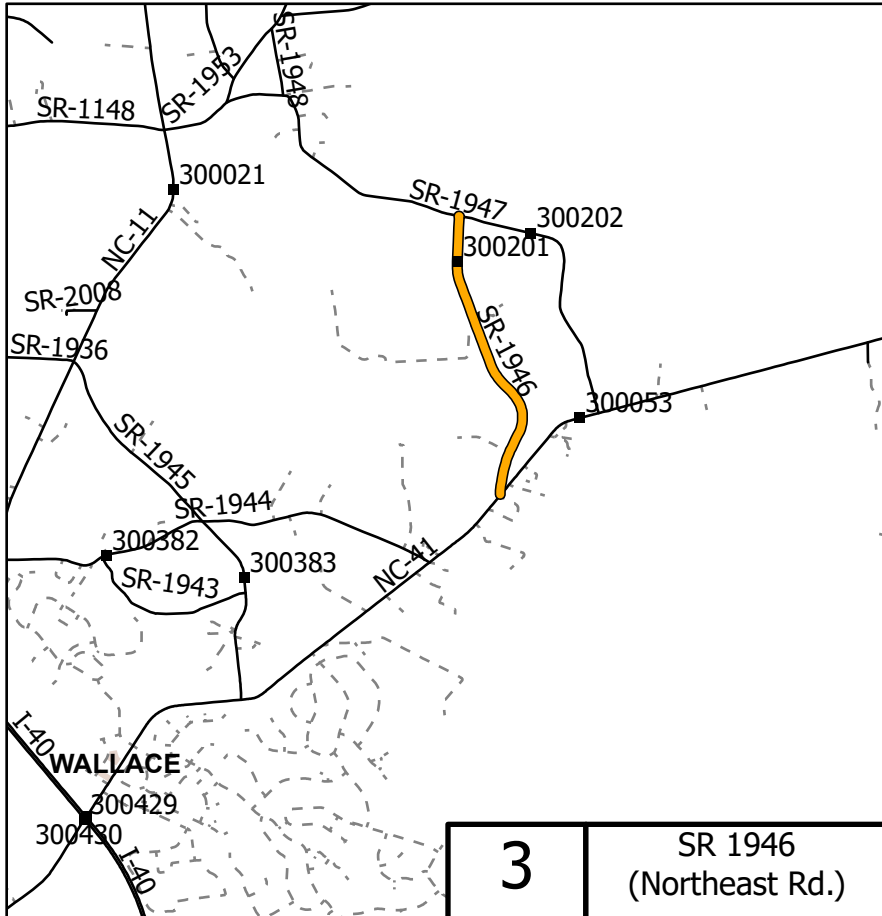
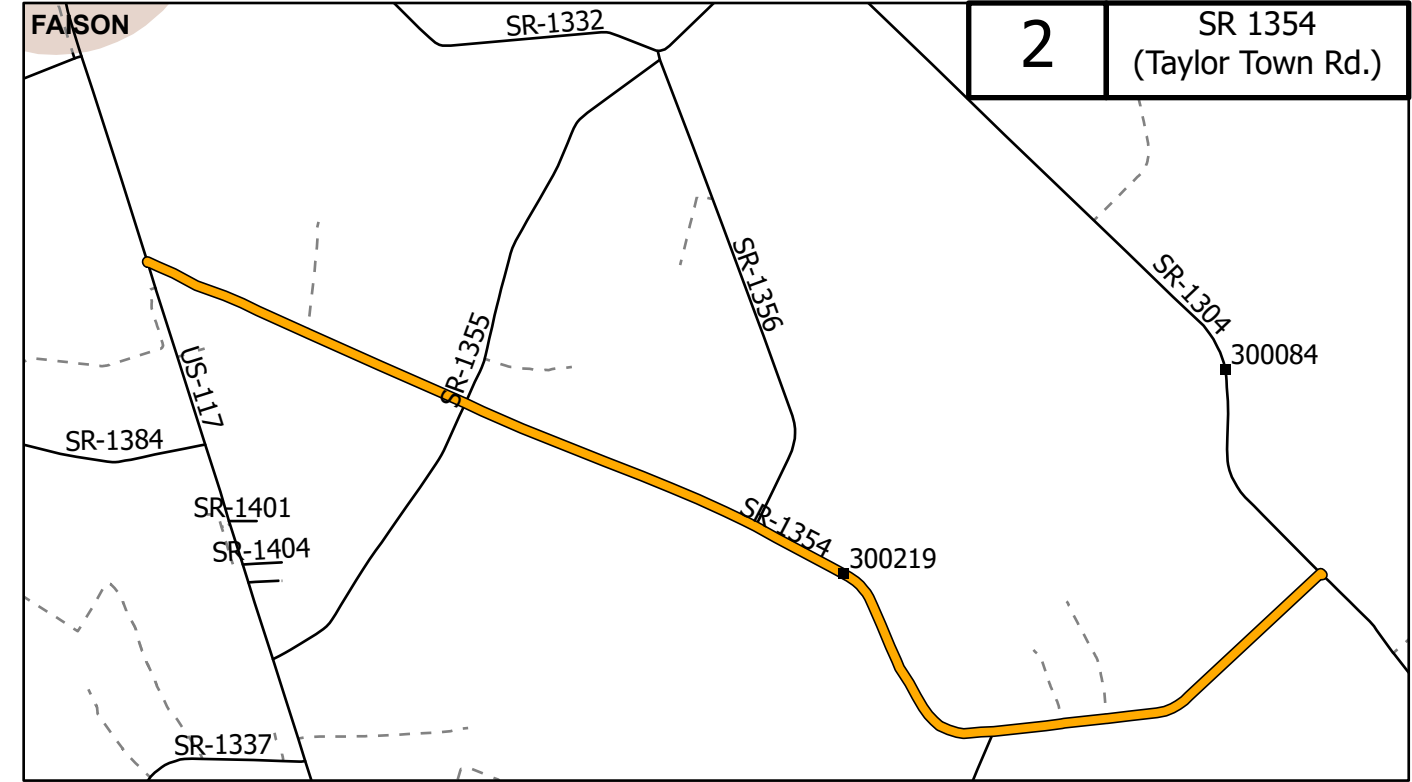
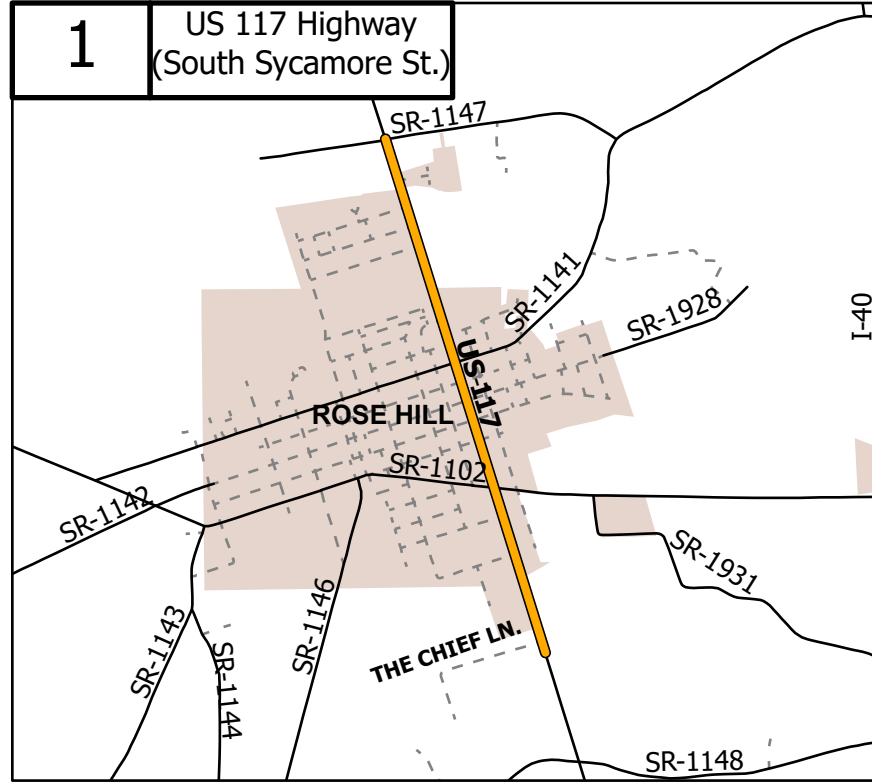
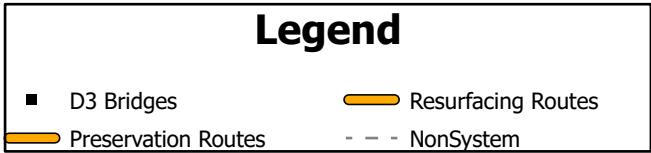
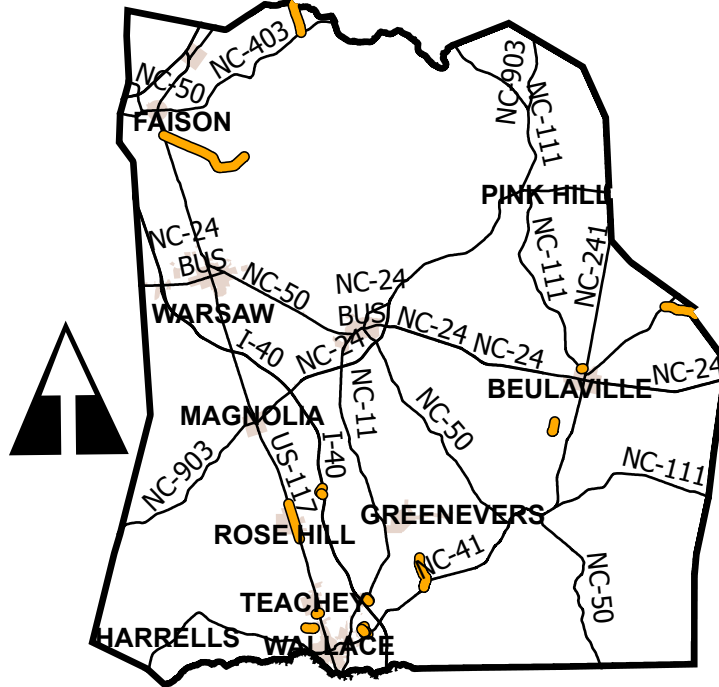


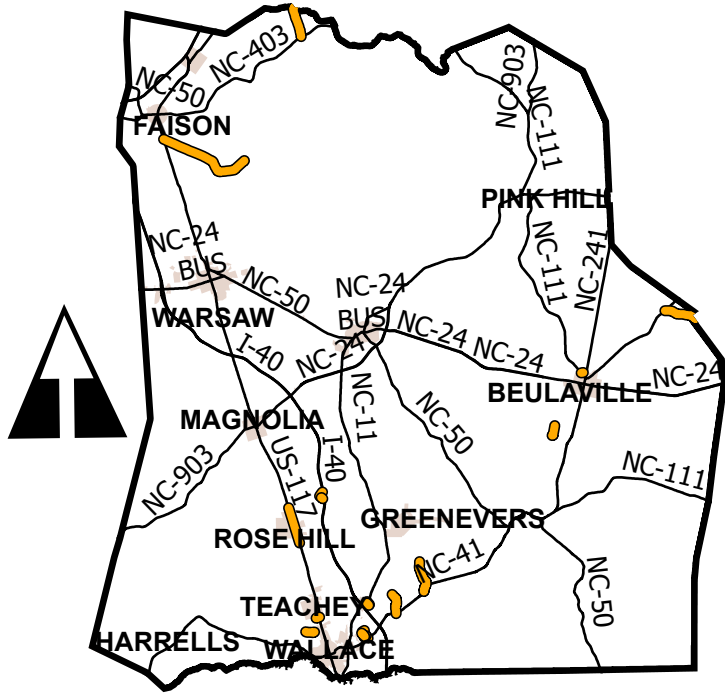
# Duplin County 1-5

PROJECT REFERENCE NO.	SHEET NO.
2023CPT.03.02.10311, ETC.	1

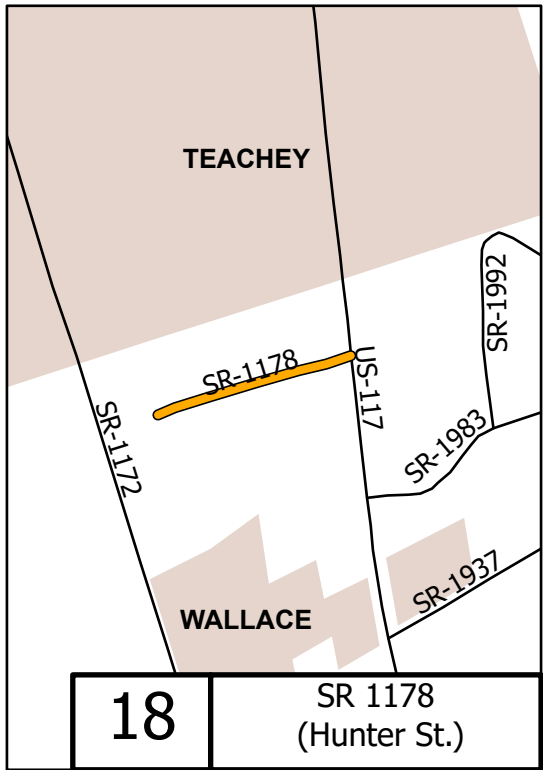
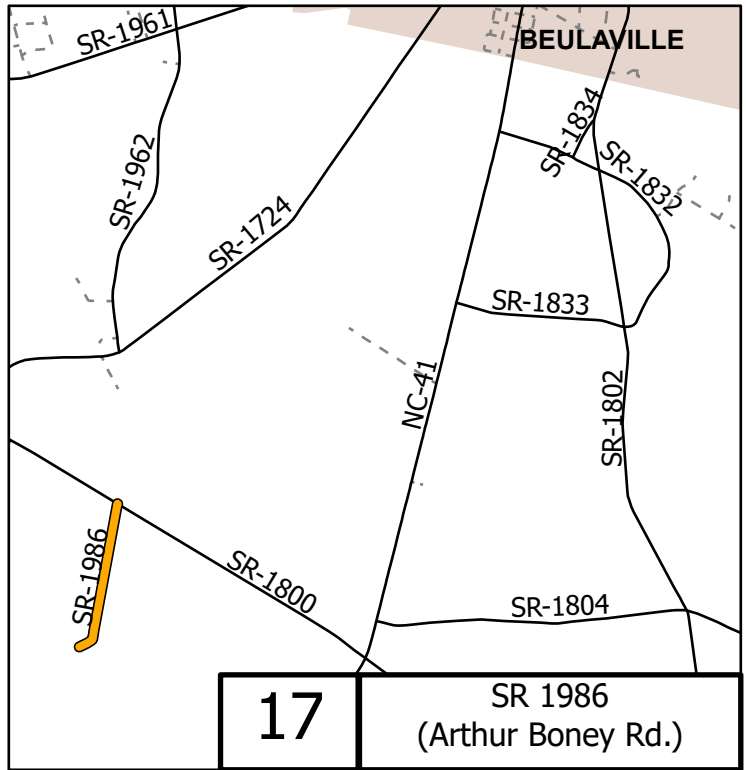
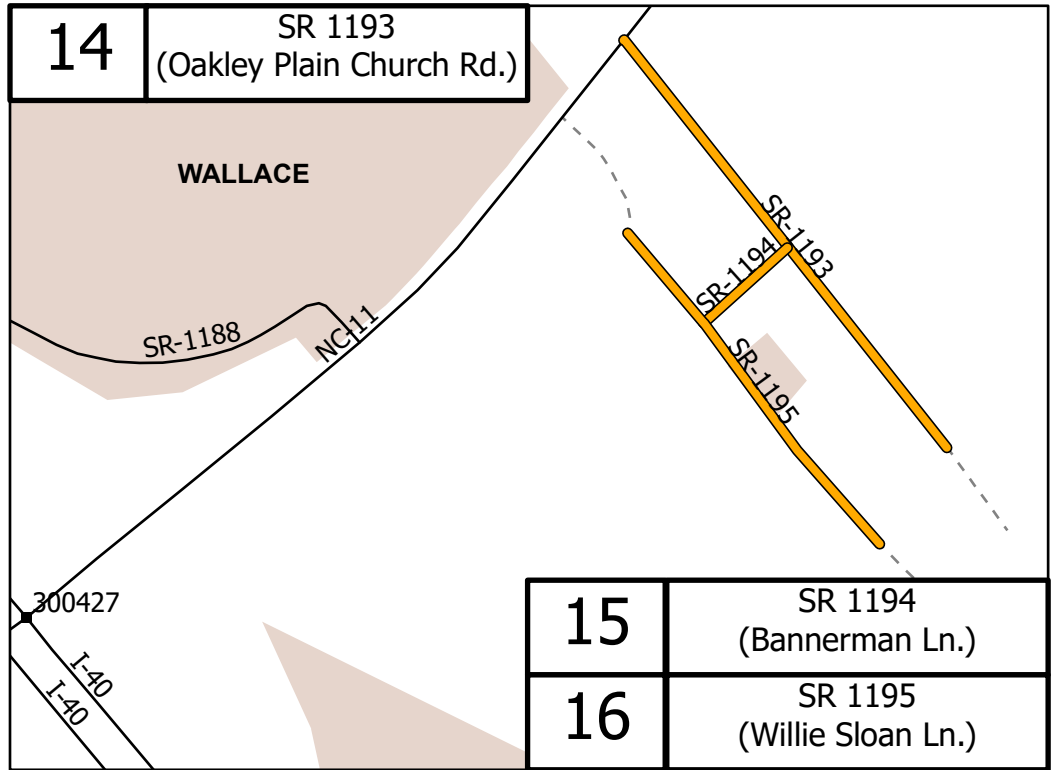
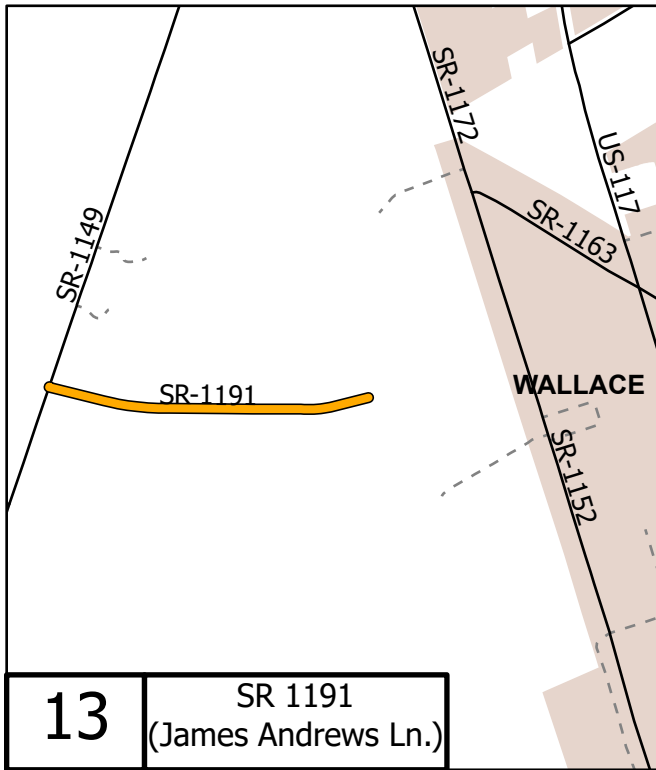
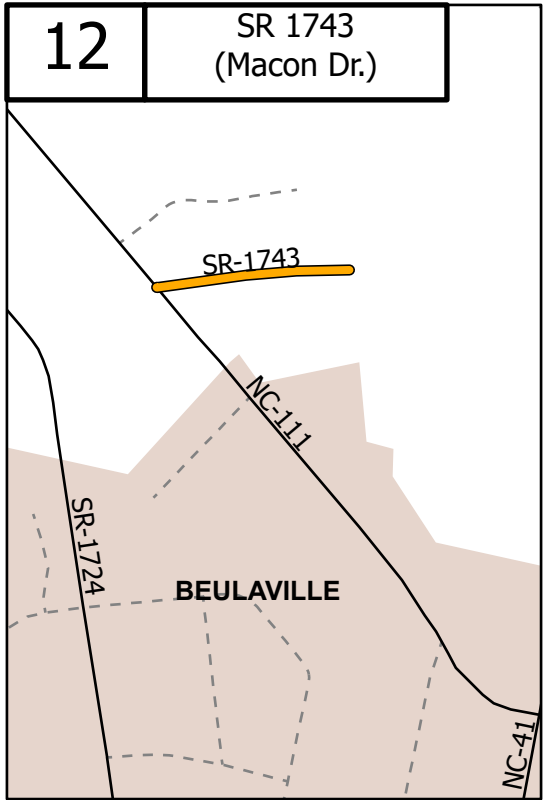
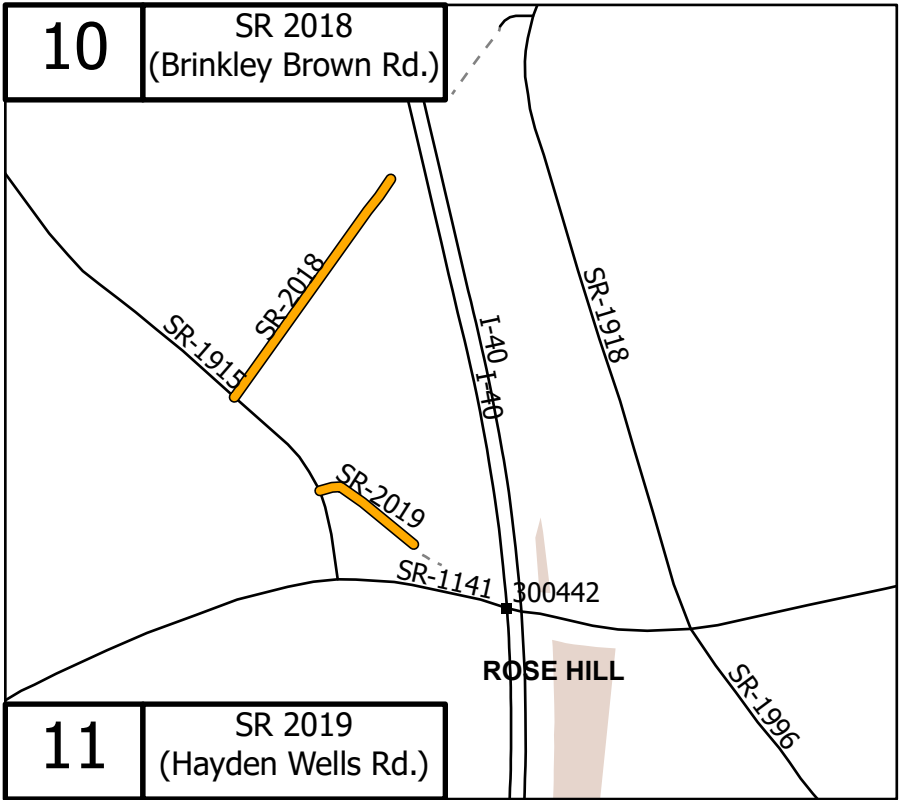
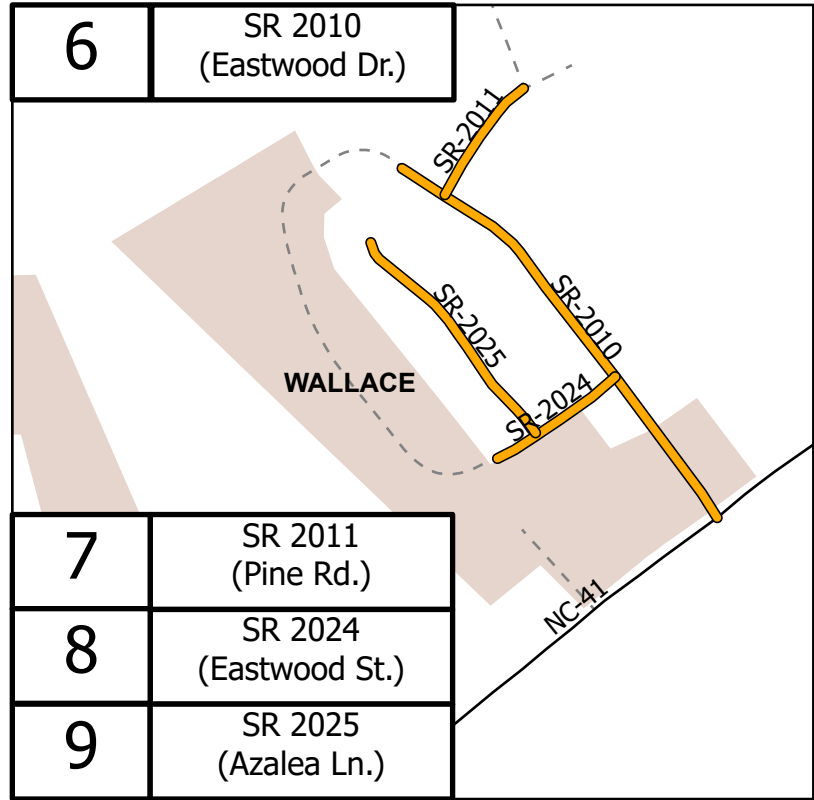


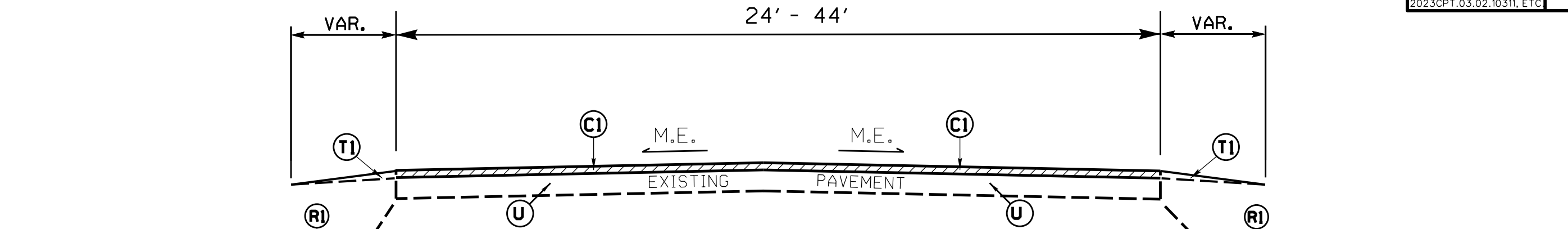
# Duplin County 6-18

PROJECT REFERENCE NO. 2023CPT.03.02.10311, ETC.	SHEET NO. 2
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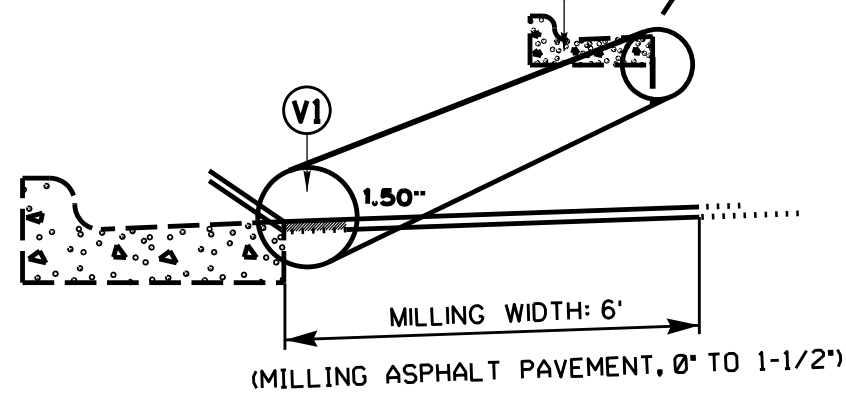
Legend	
■ D3 Bridges	— Resurfacing Routes
— Preservation Routes	- - - NonSystem



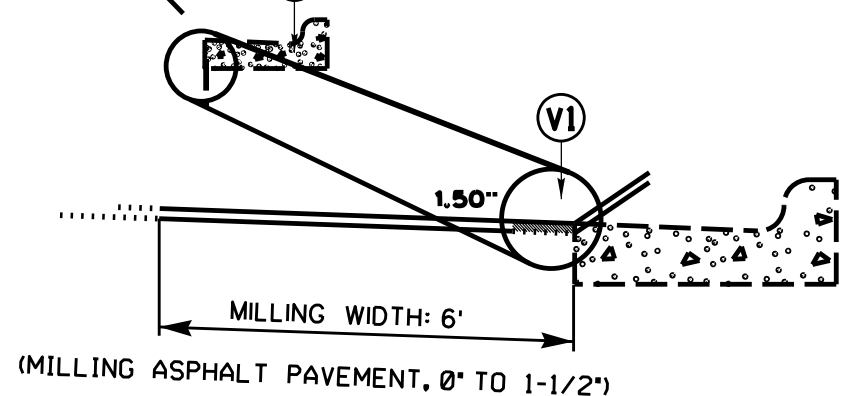


**TYPICAL SECTION NO. 1**

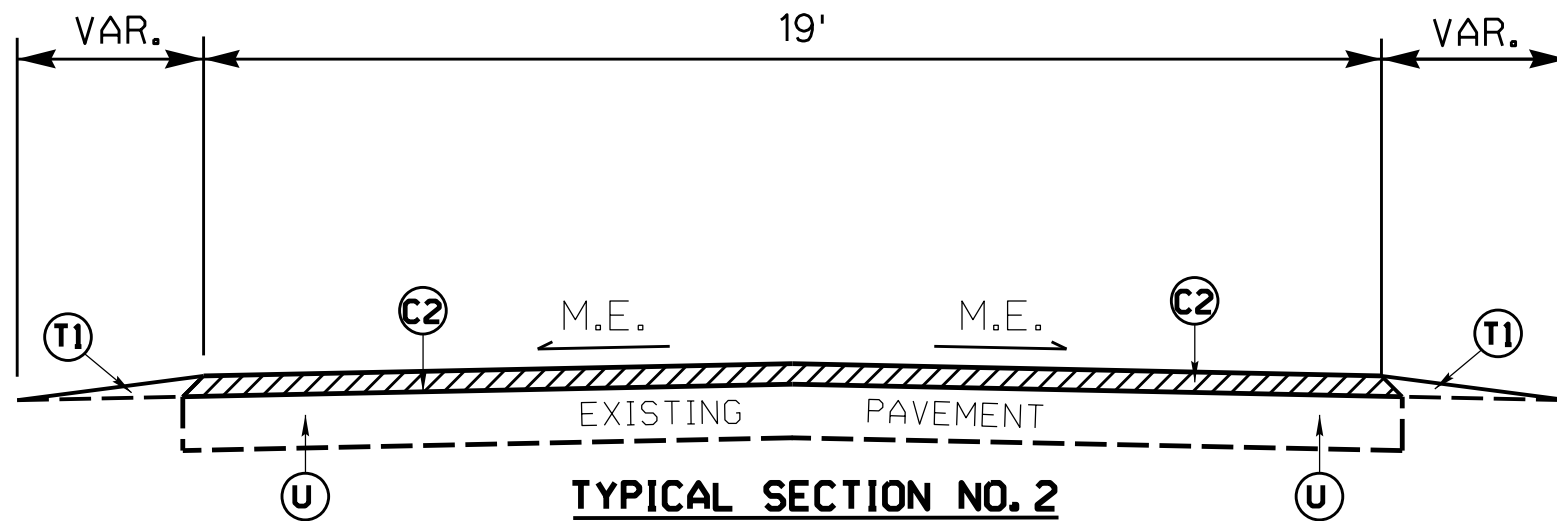
MAP NO. 1  
 US 117 HIGHWAY (S. SYCAMORE ST.)  
 MP 7.31 - MP 9.00



VI MP 7.76 - MP 8.22



VI MP 7.76 - MP 8.26



**TYPICAL SECTION NO. 2**

MAP NO. 2  
 SR 1354 (TAYLOR TOWN RD.)  
 MP 0.01 - MP 4.59  
 SEE ALSO TYPICAL NO. 3

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1½" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ.YD.
C2	PROP. APPROX. 1½" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ.YD.
C3	PROP. APPROX. 1" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 110 LBS. PER SQ.YD.
C4	PROP. APPROX. 1" DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S4.75A, AT AN AVERAGE RATE OF 110 LBS. PER SQ.YD.
F1	ASPHALT SURFACE TREATMENT - MAT COAT #67 STONE GRANITE
E1	PROP. APPROX. 5" DEPTH ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 570 LBS. PER SQ.YD.
R1	EXISTING 2'-6" CURB & GUTTER
R2	EXISTING CONCRETE BRIDGE RAIL
T1	EARTH MATERIAL (SHOULDER RECONSTRUCTION)
U	EXISTING PAVEMENT
V1	VARIABLE MILLING, 0" - 1½" DEPTH
V2	MILLING ASPHALT PAVEMENT, 1" DEPTH
V3	MILLING ASPHALT PAVEMENT, 1½" DEPTH

**NOTES:** MILL A SINGLE LANE AND PAVE BACK BY THE END OF EACH WORK DAY.

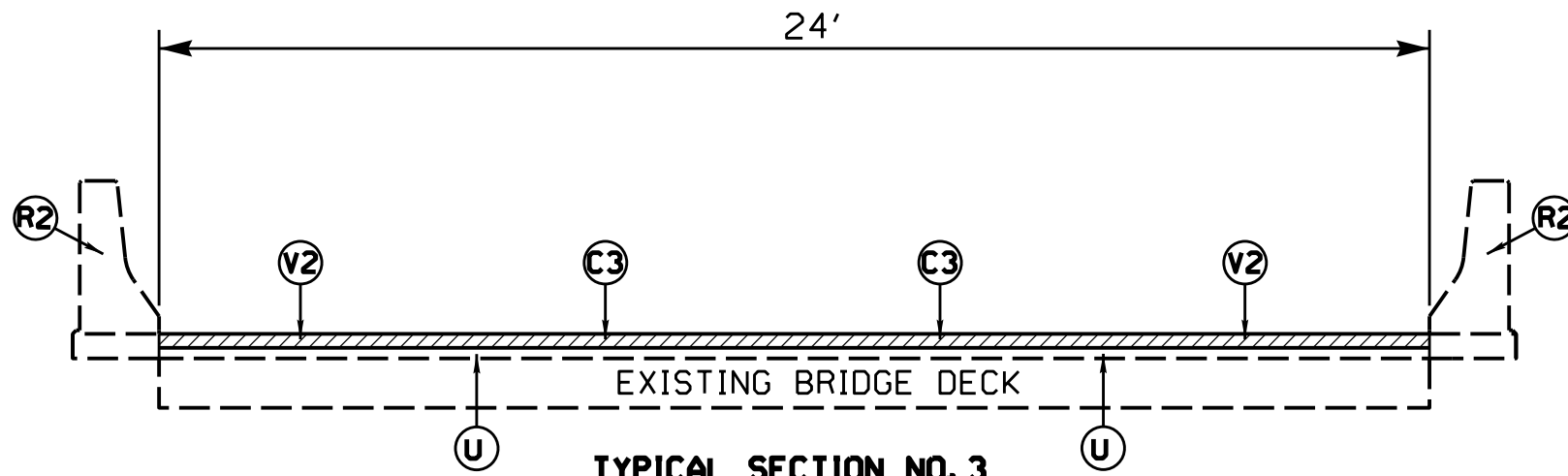
**SHOULDER WORK ON MAP NO. 1 - 4 AS NEEDED, TO BE DETERMINED BY ENGINEER.**

PAVEMENT EDGE SLOPES ARE 1:1, EXCEPT FINAL SURFACE COURSE. SEE SHOULDER WEDGE DETAIL.

REVISIONS

8/17/99

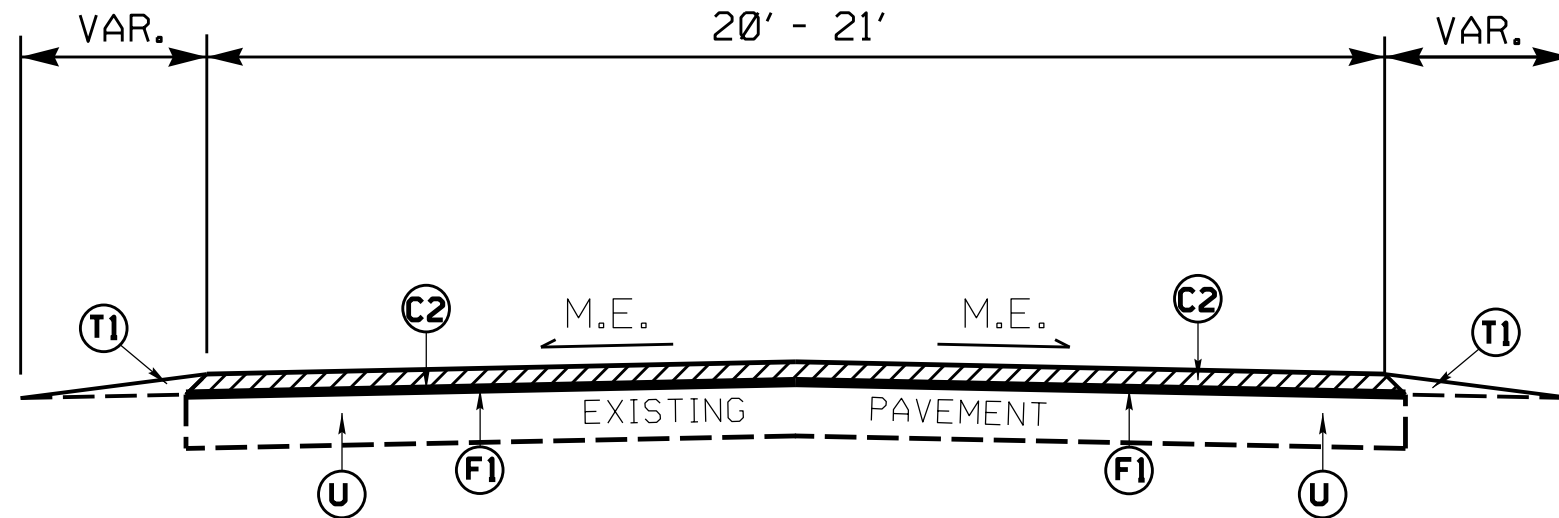
SYSTEMS DESIGN



**TYPICAL SECTION NO. 3**

MAP NO. 2  
 SR 1354 (TAYLOR TOWN RD.)  
 DUPLIN BRIDGE NO. 219  
 MP 2.04 - MP 2.09

PAVEMENT SCHEDULE	
C2	1½" S9.5B
C3	1" S9.5B
F1	MAT COAT #67 STONE GRANITE
R2	EXISTING CONCRETE BRIDGE RAIL
T1	EARTH MATERIAL SHOULDER RECONST.
U	EXISTING PAVEMENT
V2	1" MILLING
V3	1 ½" MILLING



**TYPICAL SECTION NO. 4**

MAP NO. 3  
 SR 1946 (NORTHEAST RD.)  
 MP 0.03 - MP 1.55

\*DUPLIN BRIDGE NO. 201  
 NO WORK MP 1.27 - MP 1.36

Ⓟ \*FRIS MILL 1-1/2"  
 MP 1.17 - MP 1.27  
 MP 1.36 - MP 1.55

MAP NO. 4  
 SR 1713 (KIRBY THIGPEN RD.)  
 MP 0.02 - MP 1.57

Ⓟ \*FRIS MILL 1-1/2"  
 MP 0.55 - MP 0.65

PAVEMENT EDGE SLOPES ARE 1:1, EXCEPT FINAL SURFACE COURSE. SEE SHOULDER WEDGE DETAIL.

**NOTES:** SHOULDER WORK ON MAP NO. 3 & 4 AS NEEDED, TO BE DETERMINED BY ENGINEER.

2018 ROADWAY ENGLISH STANDARD DRAWINGS

EFF. 01-16-2018  
 REV.

The following Roadway Standards as appear in "Roadway Standard Drawings" Highway Design Branch - N.C. Department of Transportation - Raleigh, N.C., Dated January, 2018 are applicable to this project and by reference hereby are considered a part of these plans:

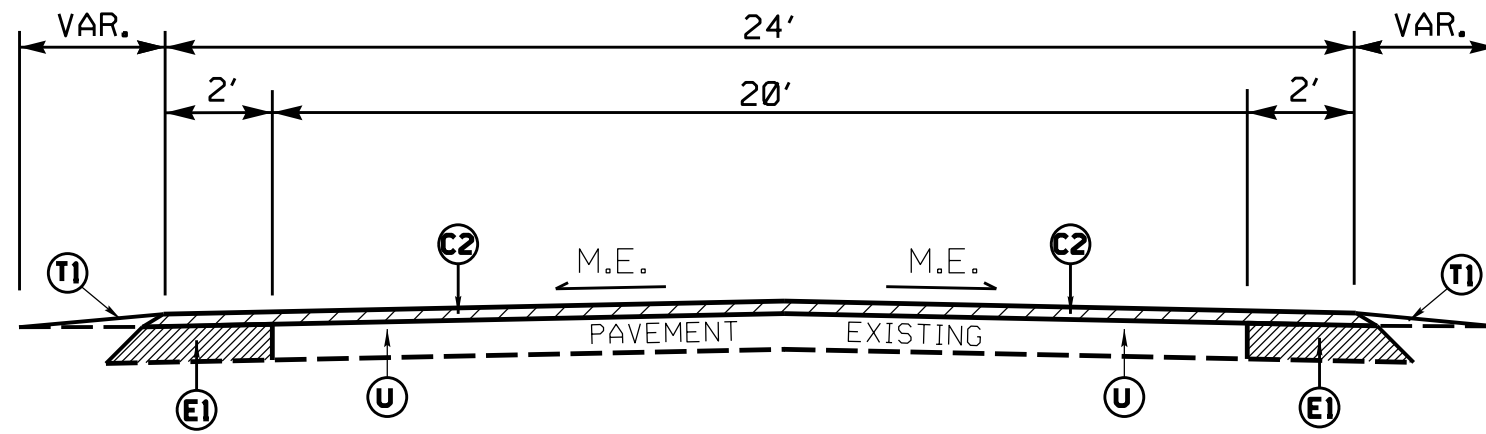
STD.NO. TITLE

DIVISION 12 - PAVT MARKING, MARKERS AND DELINEATION  
 1205.08 Pavement Markings - Symbols & Word Messages

REVISIONS

8/17/99

SYSTEMS DESIGN  
 NAME



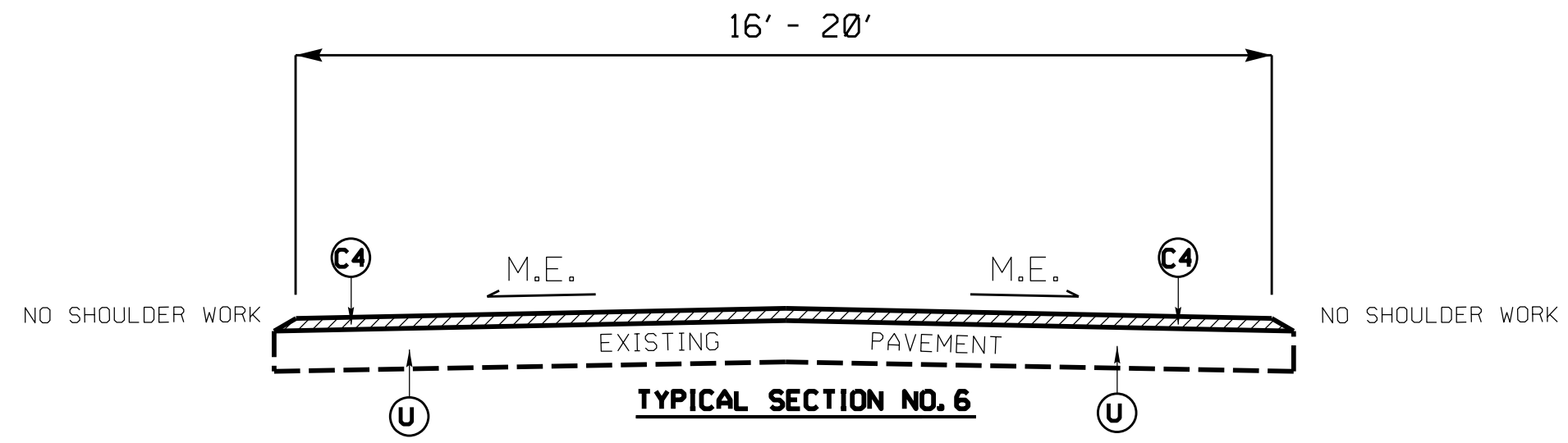
**TYPICAL SECTION NO. 5**

MAP NO. 5  
 SR 1558 (GRAHAM RD.)  
 MP 0.02 - MP 1.53

PAVEMENT SCHEDULE	
C2	1 1/2" S9.5B
C4	1" S4.75A
E1	5" B25.0C
T1	EARTH MATERIAL SHOULDER RECONST.
U	EXISTING PAVEMENT

PAVEMENT EDGE SLOPES ARE 1:1, EXCEPT FINAL SURFACE COURSE. SEE SHOULDER WEDGE DETAIL.

**NOTES:** SHOULDER WORK ON MAP NO. 5 AS NEEDED. TO BE DETERMINED BY ENGINEER.



**TYPICAL SECTION NO. 6**

MAP NO. 6  
 SR 2010 (EASTWOOD DR.)  
 MP 0.00 - MP 0.41

MAP NO. 7  
 SR 2011 (PINE RD.)  
 MP 0.00 - MP 0.12

MAP NO. 8  
 SR 2024 (EASTWOOD ST.)  
 MP 0.00 - MP 0.13

MAP NO. 9  
 SR 2025 (AZALEA LN.)  
 MP 0.00 - MP 0.22

MAP NO. 10  
 SR 2018 (BRINKLEY BROWN RD.)  
 MP 0.00 - MP 0.20

MAP NO. 11  
 SR 2019 (HAYDEN WELLS RD.)  
 MP 0.00 - MP 0.10

MAP NO. 12  
 SR 1743 (MACON DR.)  
 MP 0.00 - MP 0.11

MAP NO. 13  
 SR 1191 (JAMES ANDREWS LN.)  
 MP 0.00 - MP 0.45

MAP NO. 14  
 SR 1193 (OAKLEY PLAIN CHURCH LN.)  
 MP 0.00 - MP 0.18

MAP NO. 15  
 SR 1194 (BANNERMAN LN.)  
 MP 0.00 - MP 0.04

MAP NO. 16  
 SR 1195 (WILLIE SLOAN LN.)  
 MP 0.00 - MP 0.14

MAP NO. 17  
 SR 1986 (ARTHUR BONEY RD.)  
 MP 0.00 - MP 0.51

MAP NO. 18  
 SR 1178 (HUNTER ST.)  
 MP 0.00 - MP 0.18

REVISIONS

8/17/99  
 SYSTEMS DESIGN  
 NAME

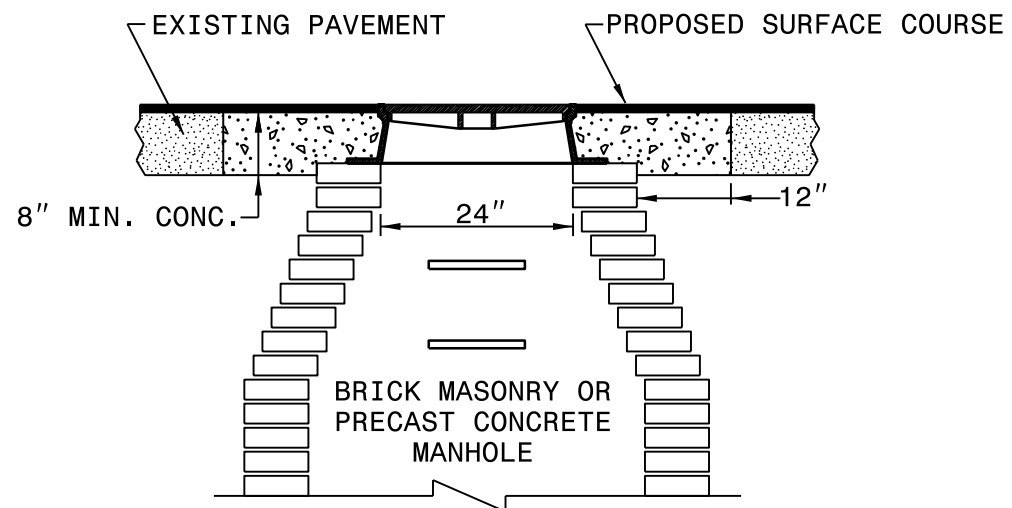
STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

ENGLISH DETAIL DRAWING FOR  
**MANHOLE AND VALVE BOX ADJUSTMENTS**

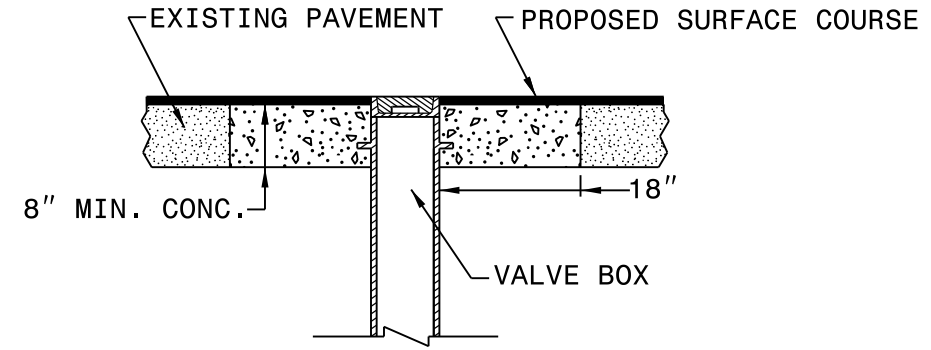
SHEET 1 OF 1  
**840D55**

GENERAL NOTES:

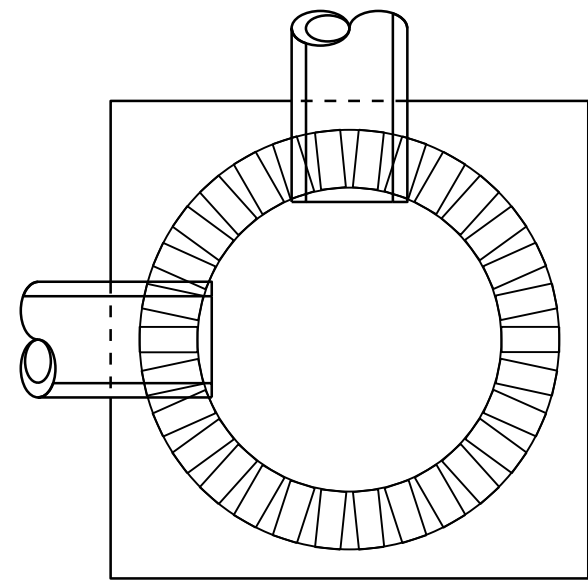
1. USE RAPID SET GROUT, MORTAR, OR CONCRETE THAT WILL TAKE FULL SET AND BECOME LOAD BEARING WITHIN SIXTY MINUTES OF PLACEMENT WITH A MINIMUM COMPRESSIVE STRENGTH OF 4000 PSI.
2. REMOVE ALL FAULTY EXISTING BRICKWORK AND REPLACE WITH NEW BRICK MASONRY.
3. SHEER CUT EXCAVATION FOR THE ADJUSTMENT ON ALL SIDES.
4. FILL AREA BELOW 8" DEPTH WITH 78M OR NO. 57 CLEAN STONE.
5. MIX MORTAR TO NCDOT SPECIFICATIONS.
6. MORTAR JOINTS  $\frac{1}{2}$ " +/-  $\frac{1}{8}$ "
7. CONSTRUCT AN ASPHALT RAMP IN ACCORDANCE WITH SECTION 858-3 OF THE 2018 STANDARD SPECIFICATIONS.



**MANHOLE CONCRETE ENCASEMENT**



**VALVE BOX CONCRETE ENCASEMENT**



**ELEVATION VIEW**

PLACE BRICK ACCORDING TO ELEVATION VIEW

SHEET 1 OF 1  
**840D55**

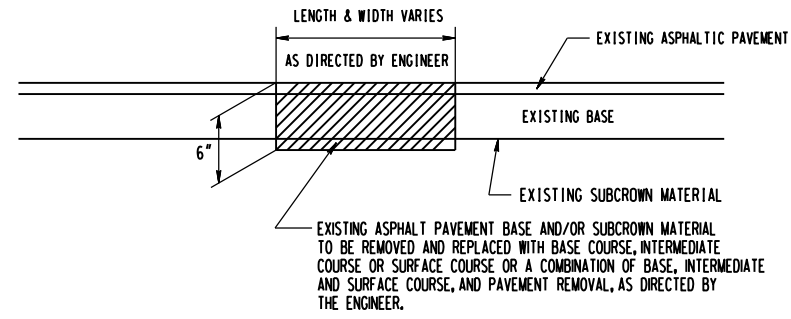
ENGLISH DETAIL DRAWING FOR  
**MANHOLE AND VALVE BOX ADJUSTMENTS**

STATE OF NORTH CAROLINA  
DEPT. OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
RALEIGH, N.C.

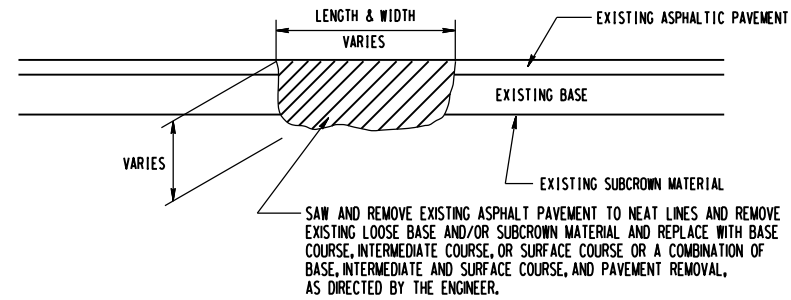
REVISIONS

28-NOV-2017 10:49  
S:\Division Resurfacing\z Design Standards & CADD\CADD\_Specs.Dwg\Tutorials\_Curb, Etc.dwg  
\$\$\$\$\$

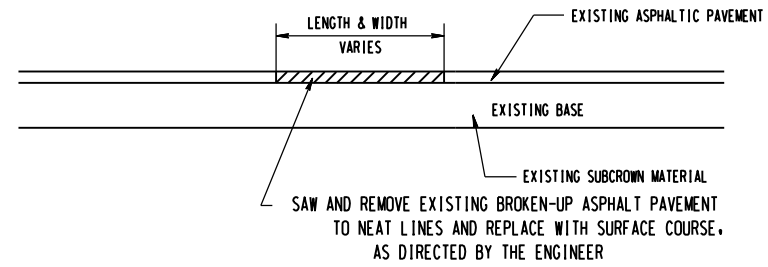
DETAILS OF REPAIRING EXISTING PAVEMENT PRIOR TO RESURFACING FOR FULL DEPTH AND MILLING



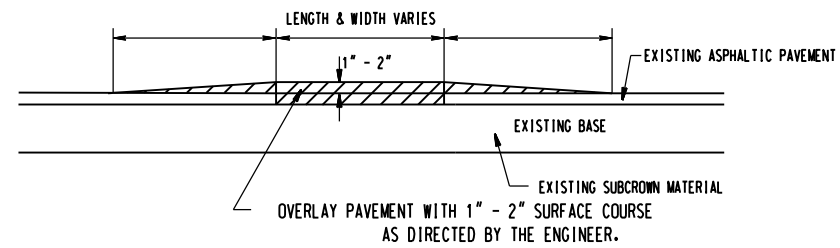
DETAIL NO. 1



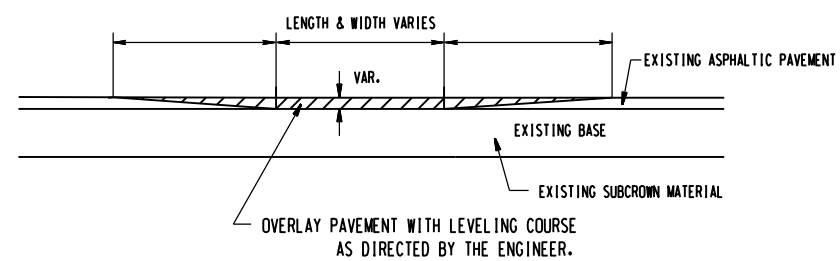
DETAIL NO. 2



DETAIL NO. 3



DETAIL NO. 4

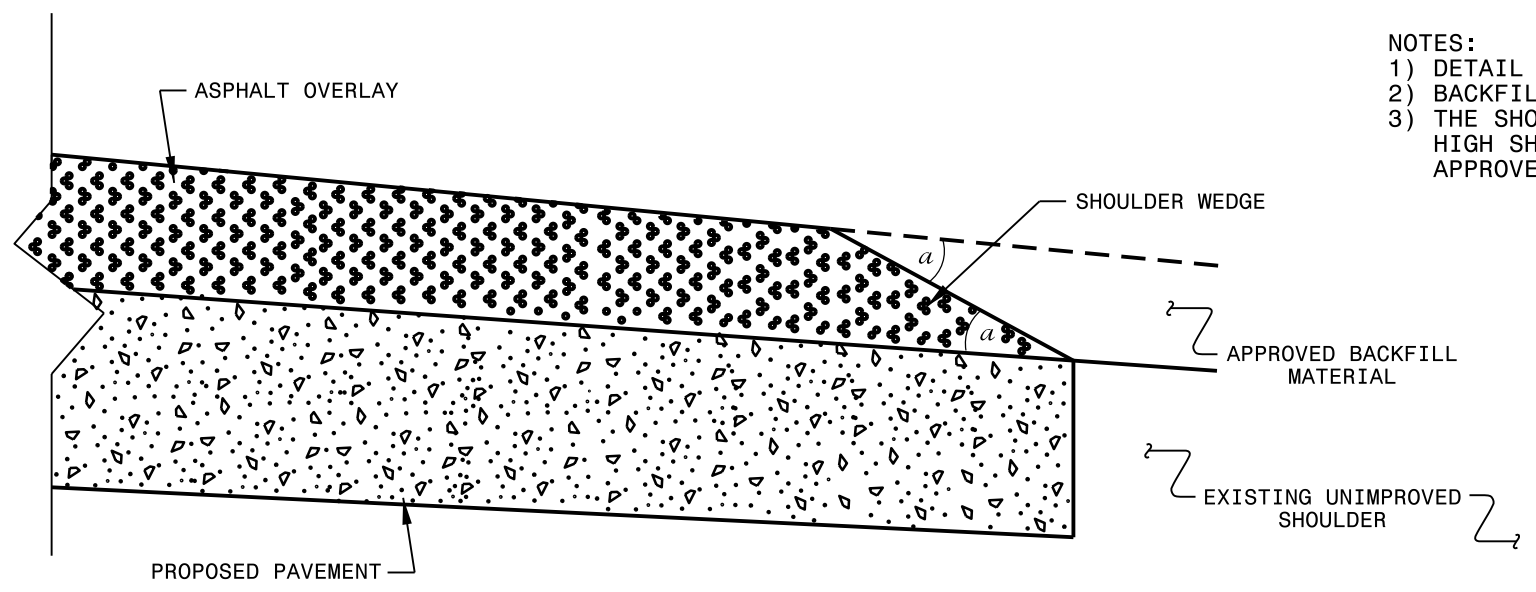


DETAIL NO. 5

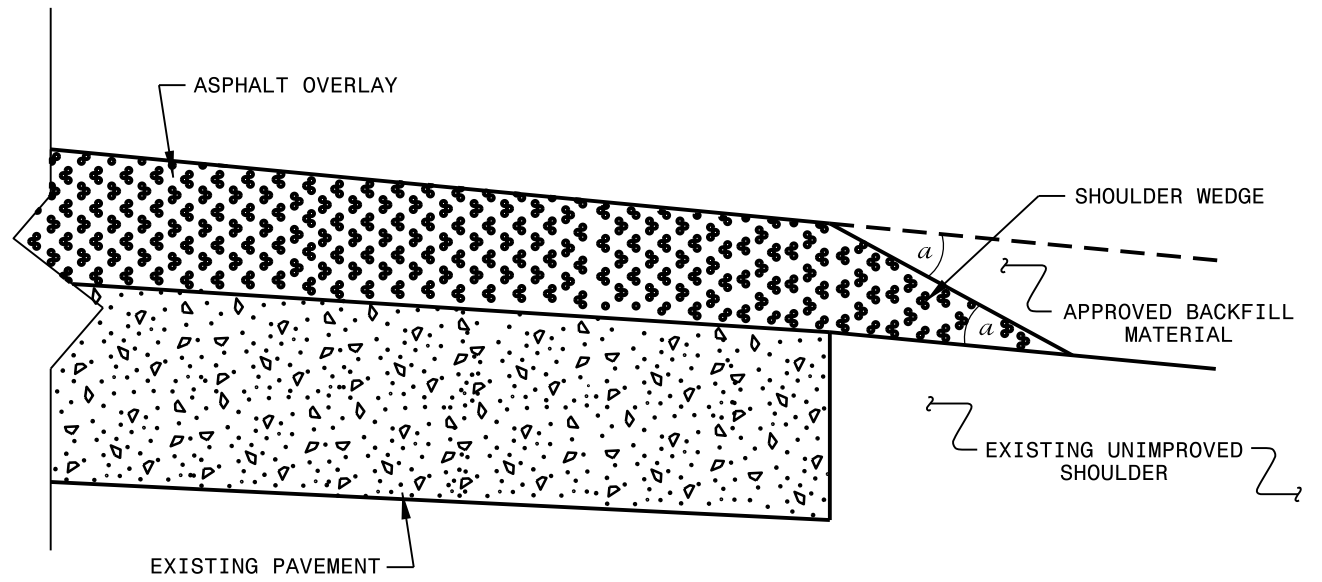
REVISIONS

20-APP-2015-1156  
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 \$\$\$SYSTRNGME\$\$\$

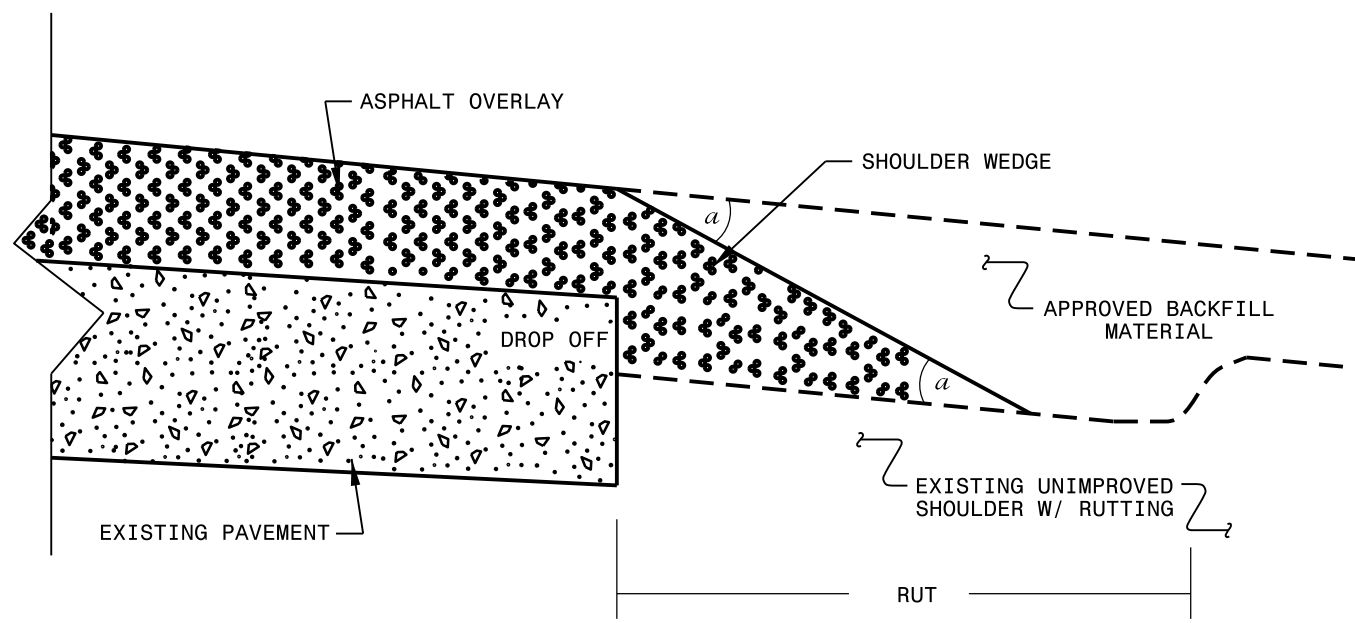
- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFC AND ULTRA-THIN BONDED WEARING COURSE.
  - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
  - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS APPROVED BY THE ENGINEER.



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ Widening or  
 with Existing Paved Shoulder having no dropoffs)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Projects w/ NO Widening)



**SHOULDER WEDGE DETAIL**  
 (Resurfacing Adjacent to  
 Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

<b>CONTRACT STANDARDS AND DEVELOPMENT UNIT</b>			
Office 919-707-6950		FAX 919-250-4119	
<b>SHOULDER WEDGE DETAILS</b>			
ORIGINAL BY: T.SPELL	DATE: 7-19-11		
MODIFIED BY:	DATE: 2/2/16		
CHECKED BY:	DATE:		
FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn			

27 JUN 2018 13:22 s:\projects\Resurfacing Projects\Division 3\Sampson August 2018\Revised Shoulder Wedge Detail.dgn  
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DIVISION OF HIGHWAYS  
STATE OF NORTH CAROLINA

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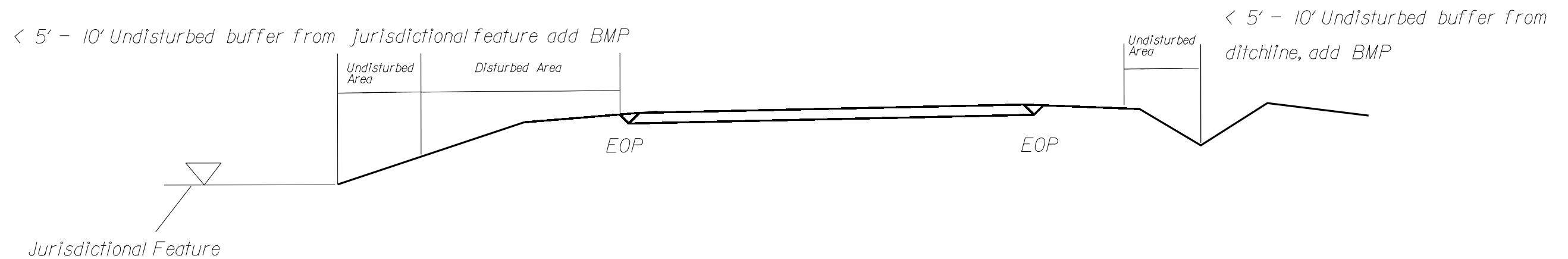
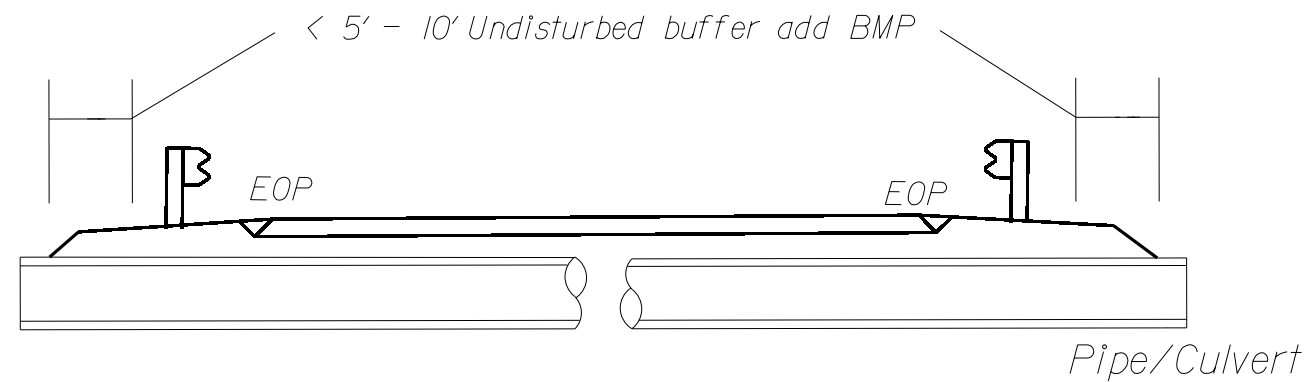
## ***SOIL STABILIZATION TIMEFRAMES***

<i>SITE DESCRIPTION</i>	<i>STABILIZATION TIME</i>	<i>TIMEFRAME EXCEPTIONS</i>
PERIMETER DIKES, SWALES, DITCHES AND SLOPES	7 DAYS	NONE
HIGH QUALITY WATER (HQW) ZONES	7 DAYS	NONE
SLOPES STEEPER THAN 3:1	7 DAYS	IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED.
SLOPES 3:1 OR FLATTER	14 DAYS	7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH.
ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1	14 DAYS	NONE, EXCEPT FOR PERIMETERS AND HQW ZONES.

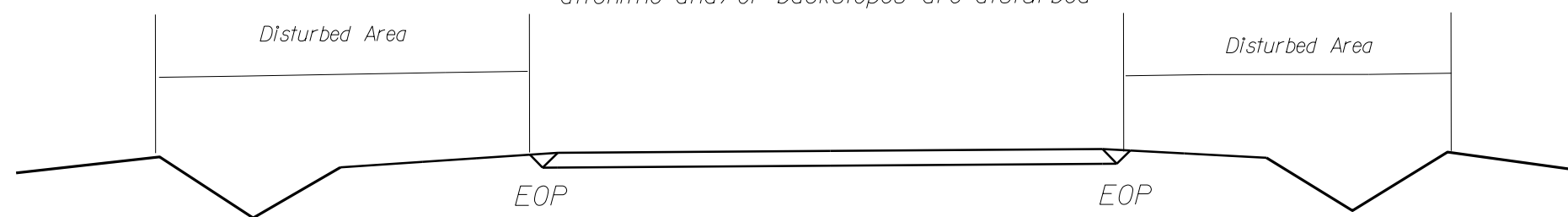
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

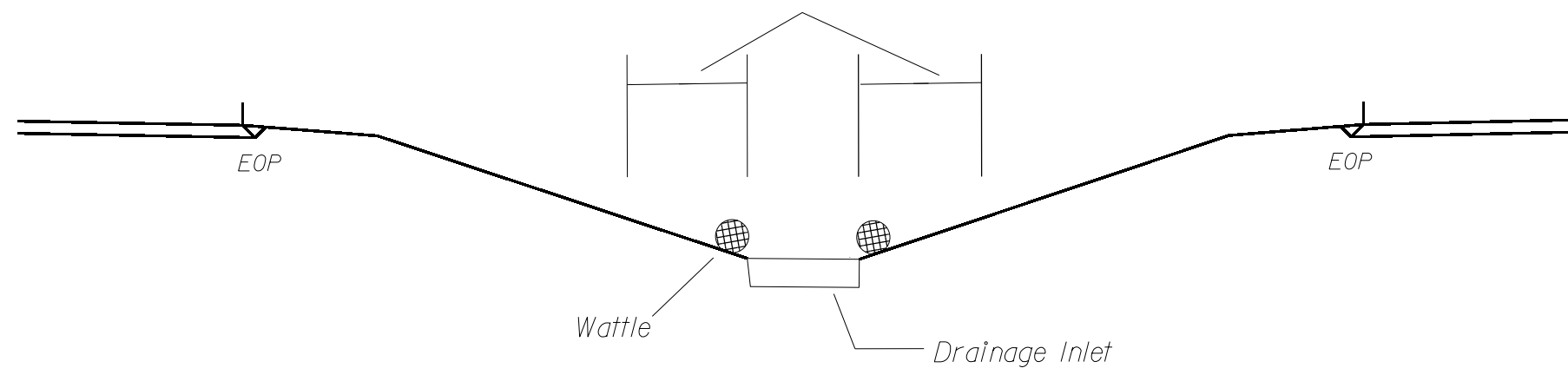
# EROSION CONTROL DETAIL



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

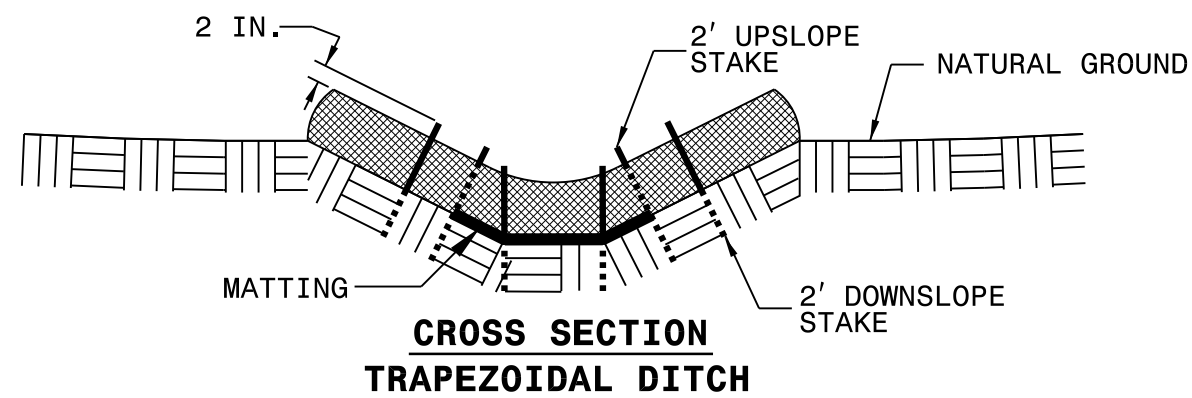
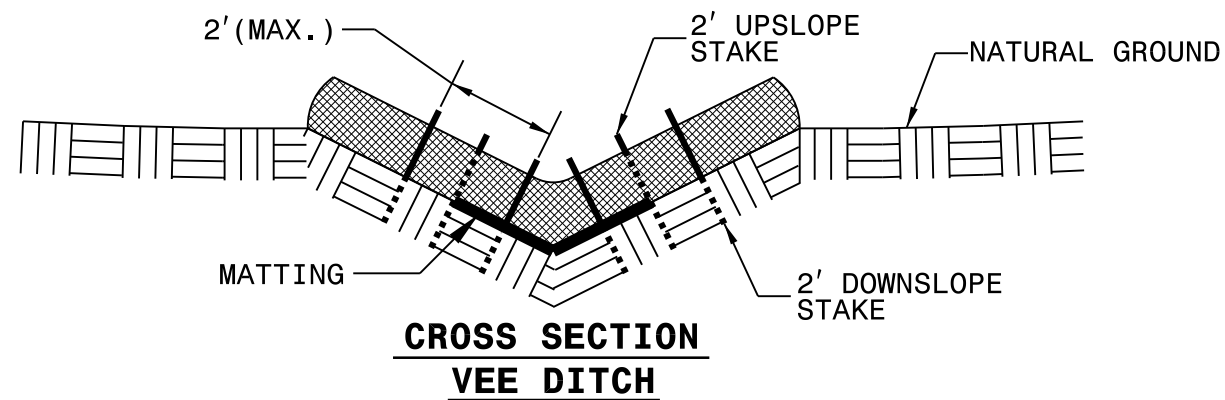
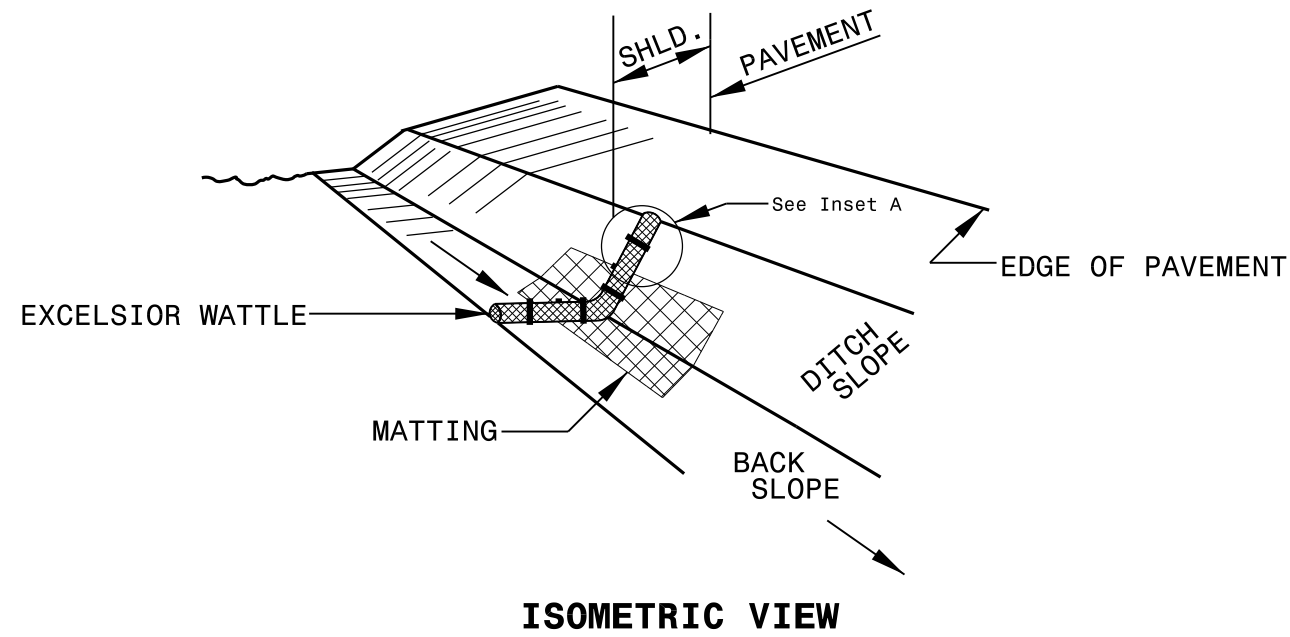


< 5' - 10' Undisturbed buffer from inlet, add wattle



NOT TO SCALE

# WATTLE DETAIL



**NOTES:**

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

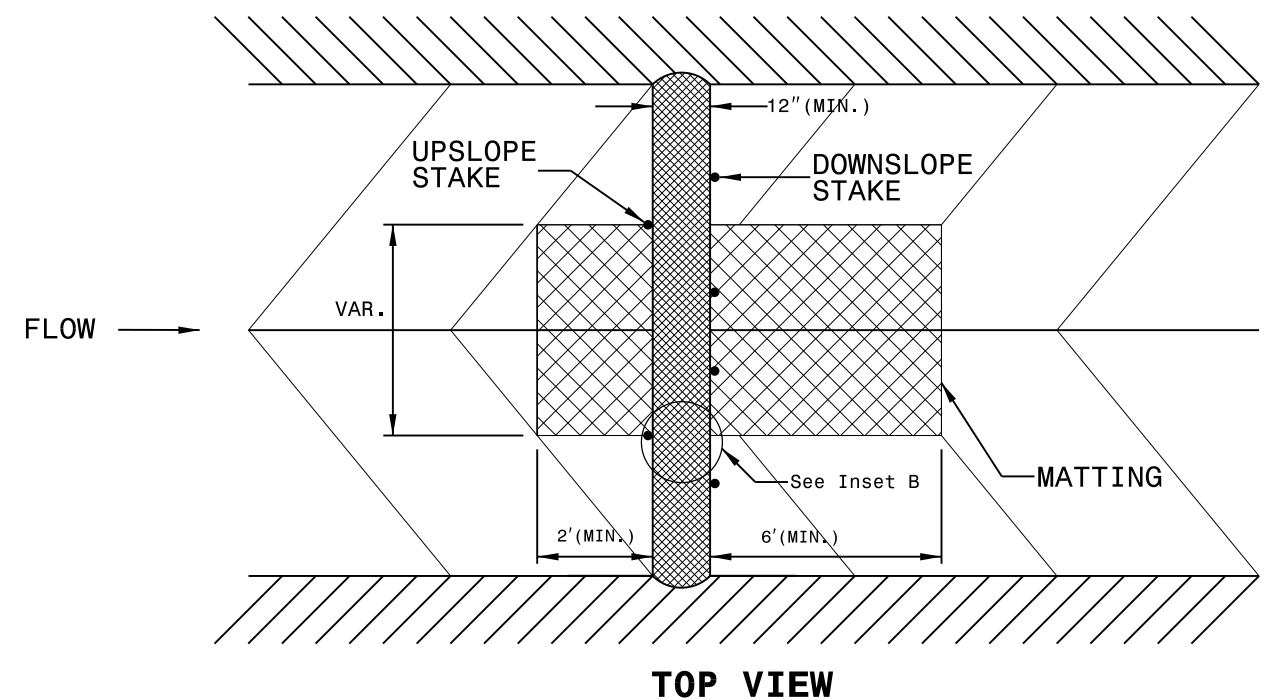
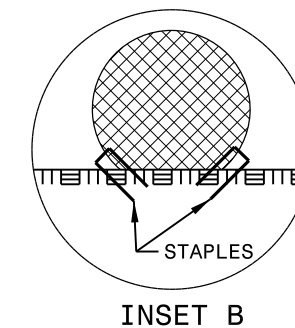
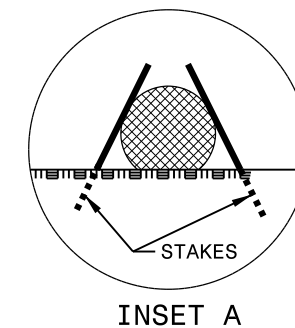
ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.





PROJECT NO.	SHEET NO.
2023CPT.03.02.10311, Etc.	10

**SUMMARY OF QUANTITIES**

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	0106000000-E	1220000000-E	1245000000-E	1297000000-E	1308000000-E	1330000000-E	1491000000-E	1519000000-E	1520000000-E	1523000000-E			
												BORROW EXCAVATION	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	1" MILLING	1.5" MILLING	0" - 1.5" MILLING	INCIDENTAL MILLING	BASE COURSE, B25.0C	SURFACE COURSE, S9.5B	LEVELING COURSE, S9.5B	SURFACE COURSE, S9.5C		
												MI	FT	CY	TON	SMI	SY	SY	SY	SY	TONS	TON	TON	TON
2023CPT.03.02.20312	Duplin	16	SR 1195 (WILLIE SLOAN LANE)	FROM BEGIN OF MAINTENANCE TO END OF MAINTENANCE [MP 0.00 - MP 0.14]	6	2	2WU	NO	NO	0.14	16													
<b>TOTAL FOR MAP NO. 16</b>											<b>0.14</b>													
2023CPT.03.02.20312	Duplin	17	SR 1986 (ARTHUR BONEY ROAD)	FROM SR 1800 (JACKSON STORE RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.51]	6	2	2WU	NO	NO	0.51	19													
<b>TOTAL FOR MAP NO. 17</b>											<b>0.51</b>													
2023CPT.03.02.20312	Duplin	18	SR 1178 (HUNTER STREET)	FROM US 117 HWY. TO END OF MAINTENANCE [MP 0.00 - MP 0.18]	6	2	2WU	NO	NO	0.18	20													
<b>TOTAL FOR MAP NO. 18</b>											<b>0.18</b>													
<b>TOTAL FOR PROJ NO. 2023CPT.03.02.20312</b>											<b>2.79</b>													
<b>GRAND TOTAL</b>											<b>13.64</b>		<b>1,600</b>	<b>232</b>	<b>20.90</b>	<b>311</b>	<b>4,517</b>	<b>4,080</b>	<b>7,144</b>	<b>1,115</b>	<b>9,443</b>	<b>70</b>	<b>3,536</b>	
															<b>4,828</b>									



PROJECT NO.	SHEET NO.
2023CPT.03.02.10311, Etc.	12

## SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	1526000000-E	1575000000-E	1705000000-E	1705500000-E	1775500000-E	1838000000-E	2759000000-N	2815000000-N	2830000000-N	2845000000-N	6000000000-E			
												SURFACE COURSE, S4.75A	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT (FULL DEPTH) GENERIC	PATCHING EXISTING PAVEMENT (MILL) GENERIC	ASPHALT SURFACE TREATMENT, MATCOAT, #67 GRANITE	EMULSION FOR ASPHALT SURFACE TREATMENT	ADJUSTMENT OF MONUMENTS (GENERIC)	ADJUSTMENT OF DROP INLET	ADJUSTMENT OF MANHOLES	ADJUSTMENT OF METER OR VALVE BOX	TEMPORARY SILT FENCE			
												MI	FT	TON	TON	TON	TON	SY	GAL	EA	EA	EA	EA	LF	
2023CPT.03.02.20312	Duplin	16	SR 1195 (WILLIE SLOAN LANE)	FROM BEGIN OF MAINTENANCE TO END OF MAINTENANCE [MP 0.00 - MP 0.14]	6	2	2WU	NO	NO	0.14	16	73	5												
<b>TOTAL FOR MAP NO. 16</b>												<b>0.14</b>		<b>73</b>	<b>5</b>										
2023CPT.03.02.20312	Duplin	17	SR 1986 (ARTHUR BONEY ROAD)	FROM SR 1800 (JACKSON STORE RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.51]	6	2	2WU	NO	NO	0.51	19	314	22												
<b>TOTAL FOR MAP NO. 17</b>												<b>0.51</b>		<b>314</b>	<b>22</b>										
2023CPT.03.02.20312	Duplin	18	SR 1178 (HUNTER STREET)	FROM US 117 HWY. TO END OF MAINTENANCE [MP 0.00 - MP 0.18]	6	2	2WU	NO	NO	0.18	20	117	8												
<b>TOTAL FOR MAP NO. 18</b>												<b>0.18</b>		<b>117</b>	<b>8</b>										
<b>TOTAL FOR PROJ NO. 2023CPT.03.02.20312</b>												<b>2.79</b>		<b>1,729</b>	<b>121</b>										
<b>GRAND TOTAL</b>																									
												<b>13.64</b>		<b>1,729</b>	<b>1,021</b>	<b>140</b>	<b>360</b>	<b>36,931</b>	<b>14,730</b>	<b>1</b>	<b>1</b>	<b>14</b>	<b>4</b>	<b>1,076</b>	





PROJECT NO.	SHEET NO.
2023CPT.03.02.10311, Etc.	14

## SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	6009000000-E	6012000000-E	6015000000-E	6018000000-E	6021000000-E	6036000000-E	6042000000-E	6071010000-E	6084000000-E	6090000000-E	6093000000-E		
												STONE FOR EC CLASS B	SEDIMENT CONTROL STONE	TEMPORARY MULCHING	SEED FOR TEMPORARY SEEDING	FERTILIZER FOR TEMPORARY SEEDING	MATTING FOR EROSION CONTROL	1/4" HARDWARE CLOTH	WATTLE	SEED & MULCHING	SEED FOR REPAIR SEEDING	FERTILIZER FOR REPAIR SEEDING		
												MI	FT	TON	TON	ACR	LBS	TON	SY	LF	LF	AC	LB	TON
2023CPT.03.02.20312	Duplin	16	SR 1195 (WILLIE SLOAN LANE)	FROM BEGIN OF MAINTENANCE TO END OF MAINTENANCE [MP 0.00 - MP 0.14]	6	2	2WU	NO	NO	0.14	16													
TOTAL FOR MAP NO. 16											0.14													
2023CPT.03.02.20312	Duplin	17	SR 1986 (ARTHUR BONEY ROAD)	FROM SR 1800 (JACKSON STORE RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.51]	6	2	2WU	NO	NO	0.51	19													
TOTAL FOR MAP NO. 17											0.51													
2023CPT.03.02.20312	Duplin	18	SR 1178 (HUNTER STREET)	FROM US 117 HWY. TO END OF MAINTENANCE [MP 0.00 - MP 0.18]	6	2	2WU	NO	NO	0.18	20													
TOTAL FOR MAP NO. 18											0.18													
TOTAL FOR PROJ NO. 2023CPT.03.02.20312											2.79													
GRAND TOTAL											13.64		270	270	10.76	539	2.70	110	543	200	10.37	539	2.70	

PROJECT NO.	SHEET NO.
2023CPT.03.02.10311, Etc.	15

## SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	611700000-N	730000000-E	730100000-E	732400000-N	744400000-E	745600000-E
												RESPONSE FOR EROSION CONTROL	UNPAVED TRENCHING (1 CONDUIT, 2 INCH)	DIRECTIONAL DRILL (1 CONDUITS, 2 INCH)	JUNCTION BOX (STANDARD SIZE)	INDUCTIVE LOOP SAWCUT	LEAD-IN CABLE (14-2)
												EA	LF	LF	EA	LF	LF
2023CPT.03.02.10311	Duplin	1	US 117 HIGHWAY (SOUTH SYCAMORE STREET)	FROM .07 MILES W. OF CHIEF LN. TO SR 1147 (YELLOW CUT RD.) [MP 7.31 TO MP 9.00]	1	4	MU	NO	NO	1.69	24 - 44	4	470	50	6	750	1,260
<b>TOTAL FOR MAP NO. 1</b>										<b>1.69</b>		<b>4</b>	<b>470</b>	<b>50</b>	<b>6</b>	<b>750</b>	<b>1,260</b>
<b>TOTAL FOR PROJ NO. 2023CPT.03.02.10311</b>										<b>1.69</b>		<b>4</b>	<b>470</b>	<b>50</b>	<b>6</b>	<b>750</b>	<b>1,260</b>
2023CPT.03.02.20311	Duplin	2	SR 1354 (TAYLOR TOWN ROAD)	FROM SR 1304 (FRIENDSHIP CHURCH RD.) TO US 117 HWY. [MP 0.01 - MP 4.59]	2, 3	2	2WU	NO	NO	4.58	19	10					
<b>TOTAL FOR MAP NO. 2</b>										<b>4.58</b>		<b>10</b>					
2023CPT.03.02.20311	Duplin	3	SR 1946 (NORTHEAST ROAD)	FROM NC 41 HWY. TO SR 1947 (HANCHEY RD.) [MP 0.03 - MP 1.55]	4	2	2WU	NO	NO	1.52	20	4					
<b>TOTAL FOR MAP NO. 3</b>										<b>1.52</b>		<b>4</b>					
2023CPT.03.02.20311	Duplin	4	SR 1713 (KIRBY THIGPEN ROAD)	FROM NC 41 HWY. TO ONSLOW CO. LINE [MP 0.02 - MP 1.57]	4	2	2WU	NO	NO	1.55	21	4					
<b>TOTAL FOR MAP NO. 4</b>										<b>1.55</b>		<b>4</b>					
2023CPT.03.02.20311	Duplin	5	SR 1558 (GRAHAM ROAD)	FROM NC 403 HWY. TO WAYNE CO. LINE [MP 0.02 - MP 1.53]	5	2	2WU	NO	NO	1.51	25	4					
<b>TOTAL FOR MAP NO. 5</b>										<b>1.51</b>		<b>4</b>					
<b>TOTAL FOR PROJ NO. 2023CPT.03.02.20311</b>										<b>9.16</b>		<b>22</b>					
2023CPT.03.02.20312	Duplin	6	SR 2010 (EASTWOOD DRIVE)	FROM NC 41 HWY. TO END OF MAINTENACE [MP 0.00 - MP 0.41]	6	2	2WU	NO	NO	0.41	20						
<b>TOTAL FOR MAP NO. 6</b>										<b>0.41</b>							
2023CPT.03.02.20312	Duplin	7	SR 2011 (PINE ROAD)	FROM SR 2010 (EASTWOOD DR.) TO END OF MAINTENANCE [MP 0.00 - MP 0.12]	6	2	2WU	NO	NO	0.12	20						
<b>TOTAL FOR MAP NO. 7</b>										<b>0.12</b>							
2023CPT.03.02.20312	Duplin	8	SR 2024 (EASTWOOD STREET)	FROM SR 2010 (EASTWOOD DR.) TO END OF MAINTENANCE [MP 0.00 - MP 0.13]	6	2	2WU	NO	NO	0.13	20						
<b>TOTAL FOR MAP NO. 8</b>										<b>0.13</b>							
2023CPT.03.02.20312	Duplin	9	SR 2025 (AZALEA LANE)	FROM SR 2024 (EASTWOOD DR.) TO CUL-DE-SAC [MP 0.00 - MP 0.22]	6	2	2WU	NO	NO	0.22	18						
<b>TOTAL FOR MAP NO. 9</b>										<b>0.22</b>							
2023CPT.03.02.20312	Duplin	10	SR 2018 (BRINKLEY BROWN ROAD)	FROM SR 1915 (SHEFFIELD RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.20]	6	2	2WU	NO	NO	0.2	20						
<b>TOTAL FOR MAP NO. 10</b>										<b>0.2</b>							
2023CPT.03.02.20312	Duplin	11	SR 2019 (HAYDEN WELLS ROAD)	FROM SR 1915 (SHEFFIELD RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.10]	6	2	2WU	NO	NO	0.1	20						
<b>TOTAL FOR MAP NO. 11</b>										<b>0.1</b>							
2023CPT.03.02.20312	Duplin	12	SR 1743 (MACON DRIVE)	FROM NC 111 HWY. TO END OF MAINTENANCE [MP 0.00 - MP 0.11]	6	2	2WU	NO	NO	0.11	19						
<b>TOTAL FOR MAP NO. 12</b>										<b>0.11</b>							
2023CPT.03.02.20312	Duplin	13	SR 1191 (JAMES ANDREWS LANE)	FROM SR 1149 (CALICO BAY RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.45]	6	2	2WU	NO	NO	0.45	20						
<b>TOTAL FOR MAP NO. 13</b>										<b>0.45</b>							
2023CPT.03.02.20312	Duplin	14	SR 1193 (OAKLEY PLAIN CHURCH LANE)	FROM NC 11 HWY. TO END OF MAINTENANCE [MP 0.00 - MP 0.18]	6	2	2WU	NO	NO	0.18	16						
<b>TOTAL FOR MAP NO. 14</b>										<b>0.18</b>							
2023CPT.03.02.20312	Duplin	15	SR 1194 (BANNERMAN LANE)	FROM SR 1193 (OAKLEY PLAIN CHURCH LN.) TO SR 1195 (WILLIE SLOAN LN.) [MP 0.00 - MP 0.04]	6	2	2WU	NO	NO	0.04	16						
<b>TOTAL FOR MAP NO. 15</b>										<b>0.04</b>							

PROJECT NO.	SHEET NO.
2023CPT.03.02.10311, Etc.	16

### SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	6117000000-N	7300000000-E	7301000000-E	7324000000-N	7444000000-E	7456000000-E
												RESPONSE FOR EROSION CONTROL	UNPAVED TRENCHING (1 CONDUIT, 2 INCH)	DIRECTIONAL DRILL (1 CONDUITS, 2 INCH)	JUNCTION BOX (STANDARD SIZE)	INDUCTIVE LOOP SAWCUT	LEAD-IN CABLE (14-2)
												EA	LF	LF	EA	LF	LF
2023CPT.03.02.20312	Duplin	16	SR 1195 (WILLIE SLOAN LANE)	FROM BEGIN OF MAINTENANCE TO END OF MAINTENANCE [MP 0.00 - MP 0.14]	6	2	2WU	NO	NO	0.14	16						
TOTAL FOR MAP NO. 16										0.14							
2023CPT.03.02.20312	Duplin	17	SR 1986 (ARTHUR BONEY ROAD)	FROM SR 1800 (JACKSON STORE RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.51]	6	2	2WU	NO	NO	0.51	19						
TOTAL FOR MAP NO. 17										0.51							
2023CPT.03.02.20312	Duplin	18	SR 1178 (HUNTER STREET)	FROM US 117 HWY. TO END OF MAINTENANCE [MP 0.00 - MP 0.18]	6	2	2WU	NO	NO	0.18	20						
TOTAL FOR MAP NO. 18										0.18							
TOTAL FOR PROJ NO. 2023CPT.03.02.20312										2.79							
GRAND TOTAL										13.64		26	470	50	6	750	1,260

PROJECT NO.	SHEET NO.
2023CPT.03.02.10311, Etc.	17

# THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4510000000-N	4685000000-E		4695000000-E	4709000000-E	4720000000-E		
								MI	FT	WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	LAW ENFORCEMENT	4" X 90 M WHITE THERMO	4" X 90 M YELLOW THERMO	8" X 90 M WHITE THERMO	24" X 90 M WHITE THERMO	THERMO MSG ONLY (90 MIL)		
								SF	LS	HR	LF	LF	LF	LF	EA				
2023CPT.03.02.10311	Duplin	1	US 117 HIGHWAY (SOUTH SYCAMORE STREET)	FROM .07 MILES W. OF CHIEF LN. TO SR 1147 (YELLOW CUT RD.) [MP 7.31 TO MP 9.00]	1	4	MU	1.69	24 - 44										
<b>TOTAL FOR MAP NO. 1</b>								<b>1.69</b>			<b>0.10</b>	<b>40</b>	<b>14,953</b>	<b>20,204</b>	<b>30</b>	<b>230</b>	<b>20</b>		
<b>TOTAL FOR PROJ NO. 2023CPT.03.02.10311</b>								<b>1.69</b>			<b>0.10</b>	<b>40</b>	<b>14,953</b>	<b>20,204</b>	<b>30</b>	<b>230</b>	<b>20</b>		
													<b>35,157</b>						
2023CPT.03.02.20311	Duplin	2	SR 1354 (TAYLOR TOWN ROAD)	FROM SR 1304 (FRIENDSHIP CHURCH RD.) TO US 117 HWY. [MP 0.01 - MP 4.59]	2, 3	2	2WU	4.58	19	512	0.27								
<b>TOTAL FOR MAP NO. 2</b>								<b>4.58</b>		<b>512</b>	<b>0.27</b>								
2023CPT.03.02.20311	Duplin	3	SR 1946 (NORTHEAST ROAD)	FROM NC 41 HWY. TO SR 1947 (HANCHEY RD.) [MP 0.03 - MP 1.55]	4	2	2WU	1.52	20		0.09								
<b>TOTAL FOR MAP NO. 3</b>								<b>1.52</b>			<b>0.09</b>								
2023CPT.03.02.20311	Duplin	4	SR 1713 (KIRBY THIGPEN ROAD)	FROM NC 41 HWY. TO ONSLOW CO. LINE [MP 0.02 - MP 1.57]	4	2	2WU	1.55	21		0.09								
<b>TOTAL FOR MAP NO. 4</b>								<b>1.55</b>			<b>0.09</b>								
2023CPT.03.02.20311	Duplin	5	SR 1558 (GRAHAM ROAD)	FROM NC 403 HWY. TO WAYNE CO. LINE [MP 0.02 - MP 1.53]	5	2	2WU	1.51	24		0.09								
<b>TOTAL FOR MAP NO. 5</b>								<b>1.51</b>			<b>0.09</b>								
<b>TOTAL FOR PROJ NO. 2023CPT.03.02.20311</b>								<b>9.16</b>		<b>512</b>	<b>0.54</b>								
2023CPT.03.02.20312	Duplin	6	SR 2010 (EASTWOOD DRIVE)	FROM NC 41 HWY. TO END OF MAINTENACE [MP 0.00 - MP 0.41]	6	2	2WU	0.41	20		0.02								
<b>TOTAL FOR MAP NO. 6</b>								<b>0.41</b>			<b>0.02</b>								
2023CPT.03.02.20312	Duplin	7	SR 2011 (PINE ROAD)	FROM SR 2010 (EASTWOOD DR.) TO END OF MAINTENANCE [MP 0.00 - MP 0.12]	6	2	2WU	0.12	20		0.02								
<b>TOTAL FOR MAP NO. 7</b>								<b>0.12</b>			<b>0.02</b>								
2023CPT.03.02.20312	Duplin	8	SR 2024 (EASTWOOD STREET)	FROM SR 2010 (EASTWOOD DR.) TO END OF MAINTENANCE [MP 0.00 - MP 0.13]	6	2	2WU	0.13	20		0.02								
<b>TOTAL FOR MAP NO. 8</b>								<b>0.13</b>			<b>0.02</b>								
2023CPT.03.02.20312	Duplin	9	SR 2025 (AZALEA LANE)	FROM SR 2024 (EASTWOOD DR.) TO CUL-DE-SAC [MP 0.00 - MP 0.22]	6	2	2WU	0.22	18		0.02								
<b>TOTAL FOR MAP NO. 9</b>								<b>0.22</b>			<b>0.02</b>								
2023CPT.03.02.20312	Duplin	10	SR 2018 (BRINKLEY BROWN ROAD)	FROM SR 1915 (SHEFFIELD RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.20]	6	2	2WU	0.2	20		0.02								
<b>TOTAL FOR MAP NO. 10</b>								<b>0.2</b>			<b>0.02</b>								
2023CPT.03.02.20312	Duplin	11	SR 2019 (HAYDEN WELLS ROAD)	FROM SR 1915 (SHEFFIELD RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.10]	6	2	2WU	0.1	20		0.02								
<b>TOTAL FOR MAP NO. 11</b>								<b>0.1</b>			<b>0.02</b>								
2023CPT.03.02.20312	Duplin	12	SR 1743 (MACON DRIVE)	FROM NC 111 HWY. TO END OF MAINTENANCE [MP 0.00 - MP 0.11]	6	2	2WU	0.11	19		0.02								
<b>TOTAL FOR MAP NO. 12</b>								<b>0.11</b>			<b>0.02</b>								
2023CPT.03.02.20312	Duplin	13	SR 1191 (JAMES ANDREWS LANE)	FROM SR 1149 (CALICO BAY RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.45]	6	2	2WU	0.45	20		0.04								
<b>TOTAL FOR MAP NO. 13</b>								<b>0.45</b>			<b>0.04</b>								

PROJECT NO.	SHEET NO.
2023CPT.03.02.10311, Etc.	18

## THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4510000000-N	4685000000-E		4695000000-E	4709000000-E	4720000000-E		
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	LAW ENFORCEMENT	4" X 90 M WHITE THERMO	4" X 90 M YELLOW THERMO	8" X 90 M WHITE THERMO	24" X 90 M WHITE THERMO	THERMO MSG ONLY (90 MIL)		
									MI	FT	SF	LS	HR	LF	LF	LF	LF	EA	
2023CPT.03.02.20312	Duplin	14	SR 1193 (OAKLEY PLAIN CHURCH LANE)	FROM NC 11 HWY. TO END OF MAINTENANCE [MP 0.00 - MP 0.18]	6	2	2WU	0.18	16		0.04								
<b>TOTAL FOR MAP NO. 14</b>									<b>0.18</b>		<b>0.04</b>								
2023CPT.03.02.20312	Duplin	15	SR 1194 (BANNERMAN LANE)	FROM SR 1193 (OAKLEY PLAIN CHURCH LN.) TO SR 1195 (WILLIE SLOAN LN.) [MP 0.00 - MP 0.04]	6	2	2WU	0.04	16		0.04								
<b>TOTAL FOR MAP NO. 15</b>									<b>0.04</b>		<b>0.04</b>								
2023CPT.03.02.20312	Duplin	16	SR 1195 (WILLIE SLOAN LANE)	FROM BEGIN OF MAINTENANCE TO END OF MAINTENANCE [MP 0.00 - MP 0.14]	6	2	2WU	0.14	16		0.04								
<b>TOTAL FOR MAP NO. 16</b>									<b>0.14</b>		<b>0.04</b>								
2023CPT.03.02.20312	Duplin	17	SR 1986 (ARTHUR BONEY ROAD)	FROM SR 1800 (JACKSON STORE RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.51]	6	2	2WU	0.51	19		0.04								
<b>TOTAL FOR MAP NO. 17</b>									<b>0.51</b>		<b>0.04</b>								
2023CPT.03.02.20312	Duplin	18	SR 1178 (HUNTER STREET)	FROM US 117 HWY. TO END OF MAINTENANCE [MP 0.00 - MP 0.18]	6	2	2WU	0.18	20		0.02								
<b>TOTAL FOR MAP NO. 18</b>									<b>0.18</b>		<b>0.02</b>								
<b>TOTAL FOR PROJ NO. 2023CPT.03.02.20312</b>									<b>2.79</b>		<b>0.36</b>								
<b>GRAND TOTAL</b>									<b>13.64</b>		<b>512</b>	<b>1.00</b>	<b>40</b>	<b>14,953</b>	<b>20,204</b>	<b>30</b>	<b>230</b>	<b>20</b>	
													<b>35,157</b>						



PROJECT NO.	SHEET NO.
2023CPT.03.02.10311, Etc.	20

## THERMOPLASTIC AND PAINT QUANTITIES

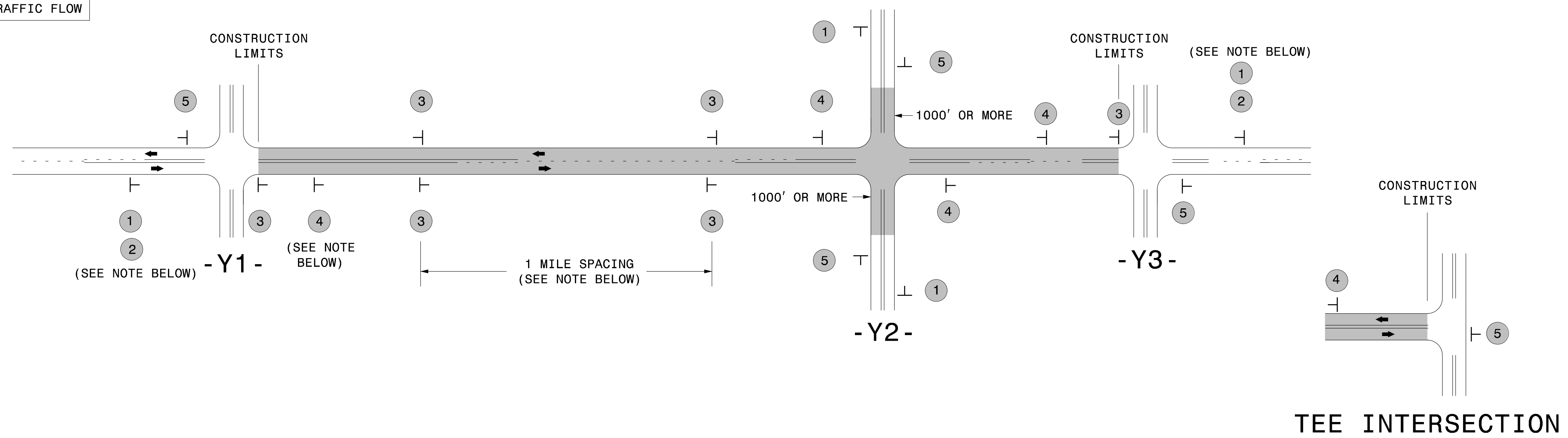
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	4725000000-E					4810000000-E		4900000000-N	4905100000-N				
										THERMO STR ARROW 90 M	THERMO LT ARROW 90 M	THERMO RT ARROW 90 M	THERMO STR & RT ARROW 90 M	THERMO STR & LT ARROW 90 M	4" WHITE PAINT	4" YELLOW PAINT	YELLOW & YELLOW MARKERS	NON CAST IRON SNOW PLOWABLE MARKERS (C/R)	NON CAST IRON SNOW PLOWABLE MARKERS (Y/Y)			
										EA	EA	EA	EA	EA	LF	LF	EA	EA	EA			
2023CPT.03.02.20312	Duplin	14	SR 1193 (OAKLEY PLAIN CHURCH LANE)	FROM NC 11 HWY. TO END OF MAINTENANCE [MP 0.00 - MP 0.18]	6	2	2WU	0.18	16													
<b>TOTAL FOR MAP NO. 14</b>									<b>0.18</b>													
2023CPT.03.02.20312	Duplin	15	SR 1194 (BANNERMAN LANE)	FROM SR 1193 (OAKLEY PLAIN CHURCH LN.) TO SR 1195 (WILLIE SLOAN LN.) [MP 0.00 - MP 0.04]	6	2	2WU	0.04	16													
<b>TOTAL FOR MAP NO. 15</b>									<b>0.04</b>													
2023CPT.03.02.20312	Duplin	16	SR 1195 (WILLIE SLOAN LANE)	FROM BEGIN OF MAINTENANCE TO END OF MAINTENANCE [MP 0.00 - MP 0.14]	6	2	2WU	0.14	16													
<b>TOTAL FOR MAP NO. 16</b>									<b>0.14</b>													
2023CPT.03.02.20312	Duplin	17	SR 1986 (ARTHUR BONEY ROAD)	FROM SR 1800 (JACKSON STORE RD.) TO END OF MAINTENANCE [MP 0.00 - MP 0.51]	6	2	2WU	0.51	19													
<b>TOTAL FOR MAP NO. 17</b>									<b>0.51</b>													
2023CPT.03.02.20312	Duplin	18	SR 1178 (HUNTER STREET)	FROM US 117 HWY. TO END OF MAINTENANCE [MP 0.00 - MP 0.18]	6	2	2WU	0.18	20													
<b>TOTAL FOR MAP NO. 18</b>									<b>0.18</b>													
<b>TOTAL FOR PROJ NO. 2023CPT.03.02.20312</b>									<b>2.79</b>													
<b>GRAND TOTAL</b>									<b>13.64</b>				<b>2</b>	<b>27</b>	<b>9</b>	<b>9</b>	<b>1</b>	<b>180,573</b>	<b>107,750</b>	<b>625</b>	<b>153</b>	<b>136</b>
													<b>48</b>		<b>288,323</b>			<b>289</b>				

# SIGNING FOR RESURFACING PROJECTS

**LEGEND**

┃ STATIONARY SIGN

← DIRECTION OF TRAFFIC FLOW



## MAINLINE (-L-) SIGNING

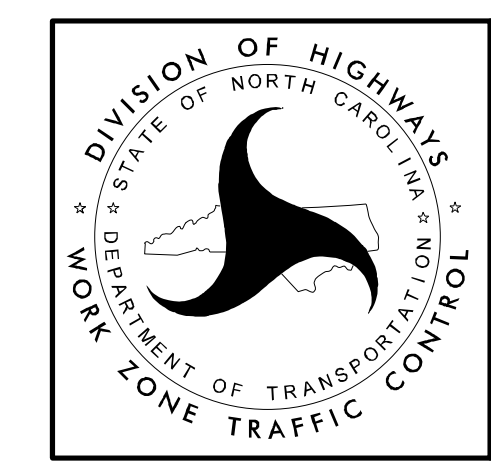
## -Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1		PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">               W20-1 48" X 48" PLACED 500' IN ADVANCE OF FLAGGER.         </div> <div style="text-align: center;">               W20-7 A 48" X 48" PLACED 250' IN ADVANCE OF FLAGGER.         </div> </div>
	2		#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3		- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.	
	4		- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.	
	5		PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.	

THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

### MAPS LESS THAN 2 MILES

FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNINGS SIGNS.



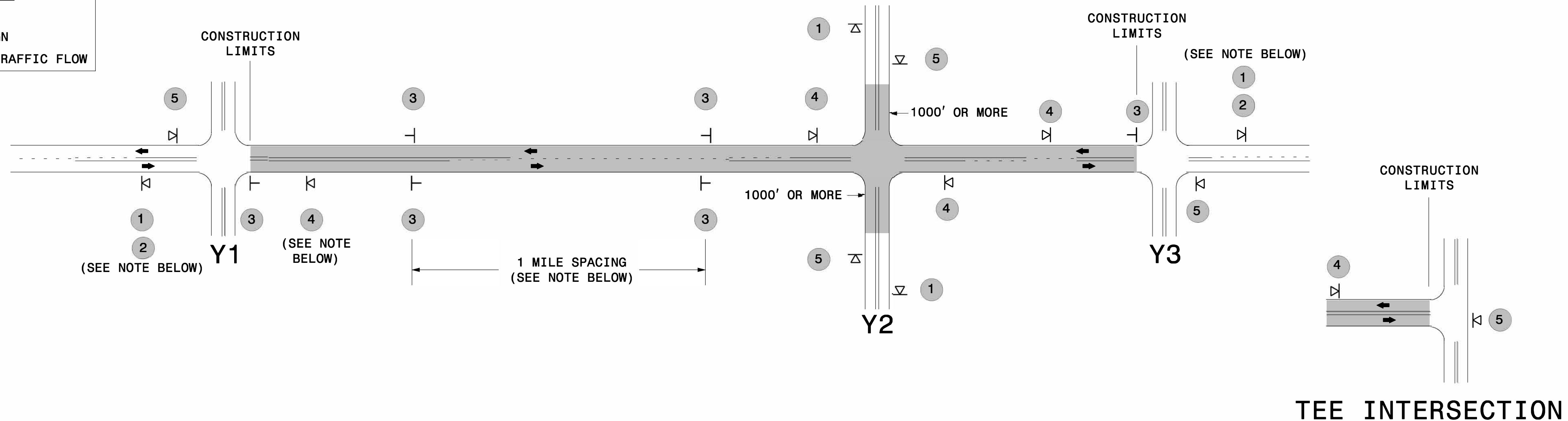
ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING



## SIGNING FOR ASPHALT SURFACE TREATMENT

**LEGEND**

⏏ PORTABLE SIGN  
 ┃ STATIONARY SIGN  
 ← DIRECTION OF TRAFFIC FLOW



### MAINLINE (-L-) SIGNING

### -Y- LINE SIGNING

<b>SIGNING NOTES AND PLACEMENT PER DIRECTION</b>	1	 	<ul style="list-style-type: none"> <li>- PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</li> <li>- SIGN #2 ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO THE NEAREST WHOLE NUMBER. DO NOT USE FRACTIONAL OR DECIMAL NUMBERS.</li> </ul>	<p><b>STATIONARY SIGNING NOT REQUIRED FOR THE FOLLOWING -Y- LINE CONDITIONS:</b></p> <ol style="list-style-type: none"> <li>1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE</li> <li>2) SUBDIVISION ROADS</li> <li>3) DEAD END ROADS</li> </ol> <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around; align-items: center;"> <div style="text-align: center;">   <small>W20-1 48" X 48"</small>            PLACED 500' IN ADVANCE OF FLAGGER.         </div> <div style="text-align: center;">   <small>W20-7 A 48" X 48"</small>            PLACED 250' IN ADVANCE OF FLAGGER.         </div> </div>
	3	 	<ul style="list-style-type: none"> <li>- ALTERNATE THE FOLLOWING TWO SIGNS:</li> <li>- STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT".</li> <li>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER.</li> <li>- AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</li> </ul>	
	4		<ul style="list-style-type: none"> <li>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</li> <li>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</li> <li>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</li> <li>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</li> </ul>	
	5		<ul style="list-style-type: none"> <li>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</li> </ul>	
	<p>THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.</p>			

**MAPS LESS THAN 2 MILES**

FOR AST RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, USE A STATIONARY "LOOSE GRAVEL" SIGN AT THE BEGINNING CONSTRUCTION LIMIT FOLLOWED BY AN "UNMARKED PAVEMENT" SIGN MIDWAY THROUGH AND AN "END ROAD WORK" SIGN AT THE END CONSTRUCTION LIMIT.



**ADVANCE WARNING SIGNS FOR 2-LANE ROADWAY ASPHALT SURFACE TREATMENT**

5/12/2017 S:\TMD\WZTC\Resurfacing\2L2W & AST Resurfacing\_Details\Resurfacing\_AdvWarn\_2Ln - AST.dgn User:hdais